

## PART E SITE SPECIFIC DEVELOPMENT

*Part E Site Specific Development* is to be read in conjunction with *State Environmental Planning Policy 65 – Design Quality of Residential Flat Development (SEPP 65)*, the associated *Apartment Design Guide* and all relevant provisions of this DCP. Where there is an inconsistency between Part E and another Part, Part E prevails to the extent of the inconsistency.

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## E1 BONDJ JUNCTION

### 1.1 INTRODUCTION

This Part applies to land as identified in Figure 1. This Part must be read in conjunction with the *Public Domain Technical Manual – Bondi Junction Centre*, and the WDCP. Where there is an inconsistency between Part E and another Part of this DCP, Part E prevails to the extent of the inconsistency.

All development is to comply with *Part B12 Design Excellence*.



Figure 1 Bondi Junction Centre

## 1.2 URBAN FORM CONTROLS

### Objectives

- (a) To ensure that Bondi Junction Centre is a vibrant and attractive commercial area.
- (b) To define the desired future character and urban form for Bondi Junction Centre.
- (c) To reinforce the role of Bondi Junction as a Strategic Centre as identified by the NSW Government.
- (d) To promote built form that increases access to natural ventilation and lighting.
- (e) To coordinate building massing along streets and across blocks.
- (f) To ameliorate the effects of existing unevenly scaled and massed buildings.
- (g) To mitigate the visual effect of tall buildings on the street.
- (h) To mitigate environmental effects of tall buildings on existing surrounding low scale residential development.
- (i) To ensure the streetscape setting for heritage buildings and other noteworthy buildings is retained and enhanced.
- (j) To create diversity within the Bondi Junction Skyline.

### Controls

- (a) Development must be sensitive to the streetscape character and views. A streetscape and context analysis is to be provided in accordance with Part B12 Design Excellence.
- (b) A lower 2/3 storey shop front façade is required along Oxford Street and Bronte Road, as identified in Figure 12, and a 6 storey street wall is required on all other streets.
- (c) Above the block edge form a tower building form is required. This form is to be set back from the street edge and from the front, side and rear boundaries (refer to Figures 13 and 14).
- (d) Towers must be slender so as to:
  - (i) Facilitate cross ventilation;
  - (ii) Provide high quality amenity to occupants of the building;
  - (iii) Encourage view corridors;
  - (iv) Provide greater solar access to public spaces and other buildings; and
  - (v) Clearly differentiate between the podium and tower elements.



### 1.3 BUILDING USE

#### Objectives

- (a) To promote street level activity.
- (b) To reinforce the primary role of Bondi Junction as a Commercial Centre through high quality commercial development.
- (c) To retain lower levels of buildings for commercial and retail uses.
- (d) To increase the diversity and range of shopping and recreational opportunities for people who live, work and visit the Centre.
- (e) To enhance community safety by increasing activity in the public domain on week nights and on weekends.
- (f) To encourage a variety of uses.
- (g) To minimise conflicts between commercial and residential uses.

#### Controls

- (a) Comply with Figure 2 for Shopping Street Hierarchy locations.

#### *Primary shopping streets*

- (b) The Ground Floor of buildings along primary shopping streets must be designed and used for retail purposes.
- (c) The First Floor of buildings along primary shopping streets must be designed and used for commercial purposes but not limited to retail.

#### *Secondary shopping streets*

- (d) The Ground Floor of buildings located on secondary shopping streets must be designed and used for commercial purposes. Retail uses are preferred
- (e) The First Floor of buildings located on secondary shopping streets must be designed and used for commercial purposes.

#### *Laneways*

- (f) Retail and commercial frontages are encouraged along laneways where possible.
- (g) Laneway uses are not to interfere with services and vehicle access.

#### *Arcades, squares and through site links*

- (h) The Ground Floor must be designed and used for retail purposes.
- (i) The First Floor must be designed and used for commercial purposes.

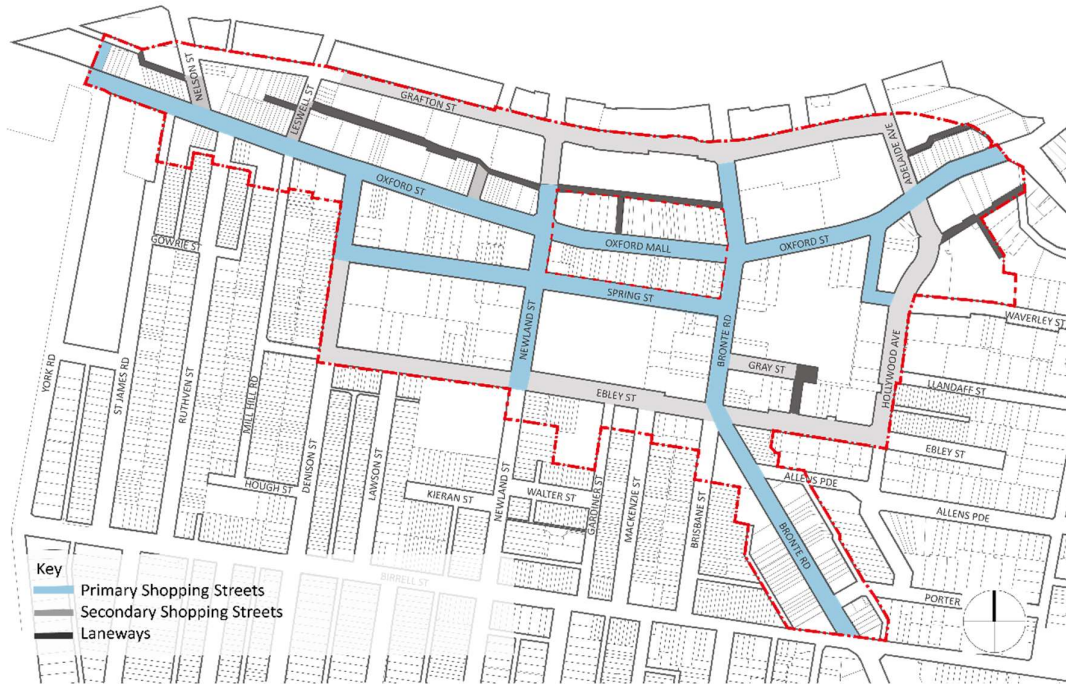


Figure 2 Street Hierarchy

## 1.4 ACCESS AND MOVEMENT

### 1.4.1 Arcades, Through Site Links and Squares

#### Objectives

- To develop a comprehensive, compact, easy to follow, safe and accessible pedestrian network.
- To create pedestrian links through large developments to provide a fine grain pedestrian network.
- To ensure that private development does not diminish the public nature of streets and laneways at ground.
- To ensure that arcades are safe.
- To expand and enhance the public domain and promote pedestrian activity throughout the centre.
- To increase active street frontages and retail uses throughout the centre.

#### Controls

- Arcades, through site links and squares are to comply with the provisions of *Part B16 Public Domain*.
- Retain and provide arcades and through site links as shown in Figure 3.
- New arcades and through site links in addition to those shown in Figure 3 are encouraged.



**Figure 3** Preferred through site links, arcades and squares

### 1.4.2 Vehicular and Service Access to Lots

#### Objectives

- To locate and design vehicle and service entries to promote active frontages, pedestrian safety and undisturbed pedestrian movement.
- To ensure that car parking is not visible from the street.
- To ensure that the building facade and active frontages are the dominant streetscape element on all streets.
- To limit the number of car park entry points to a development.
- To minimise the size and quantity of vehicle and service crossings.

#### Controls

- Comply with Figure 4 for locations for vehicular and service access.
- Car park entries and exits must not occur along primary shopping streets.
- All car park entries onto streets and laneways are to be enclosed by entry gates, roller doors or the like located in alignment with the street boundary.

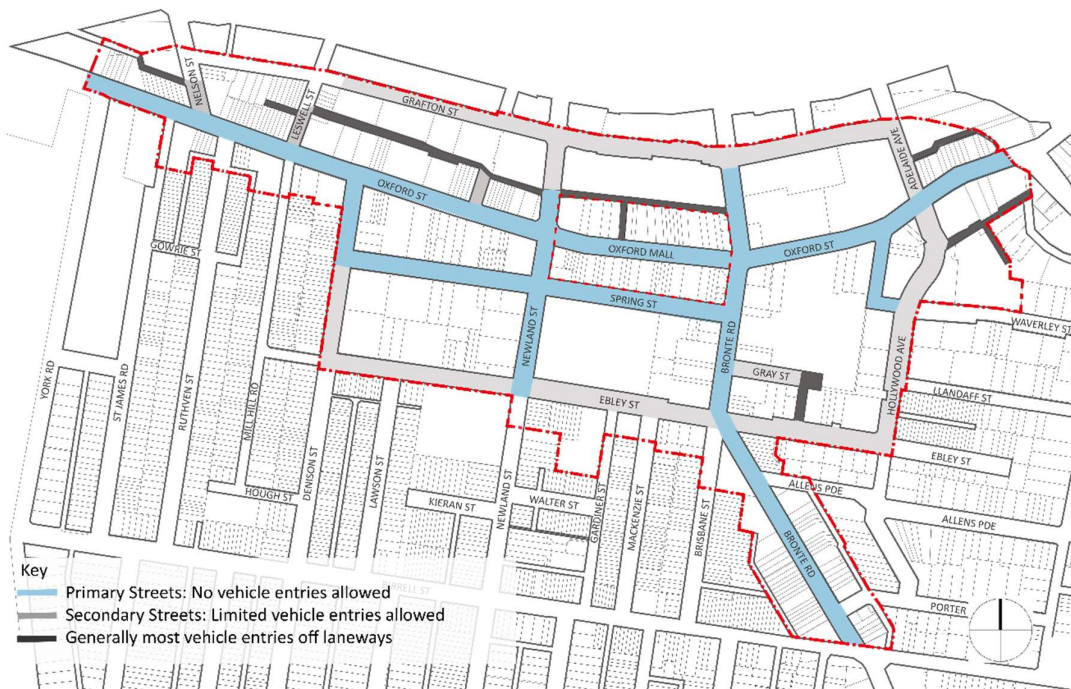


Figure 4 Vehicular and service access

### 1.4.3 Pedestrian Overpasses and Underpasses

#### Objectives

- (a) To protect and enliven streets by ensuring people circulate at street level.
- (b) To protect street level as the primary retail and commercial space of the centre.
- (c) To protect view corridors along streets.
- (d) To avoid overshadowing on streets.

#### Controls

- (a) Underpasses under public streets and laneways are not permitted unless linking directly into the public transport interchange.
- (b) Overpasses over public streets and laneways are not permitted.

### 1.4.4 On-Site Parking

#### Objectives

- (a) To avoid compromising street character, building quality, pedestrian amenity and safety through car parking.
- (b) To provide adequate space for parking and maneuvering of vehicles (including service vehicles and bicycles).
- (c) To recognise the complementary use and benefit of public transport and non-motorised modes of transport such as cycling and walking.

#### Controls

- (a) Car parks, car parking structures, vehicular maneuvering areas, private parking bays, loading docks and the like are generally to be located underground. Where this cannot be achieved due to topographic constraints, a maximum protrusion of 1.2m is permissible.

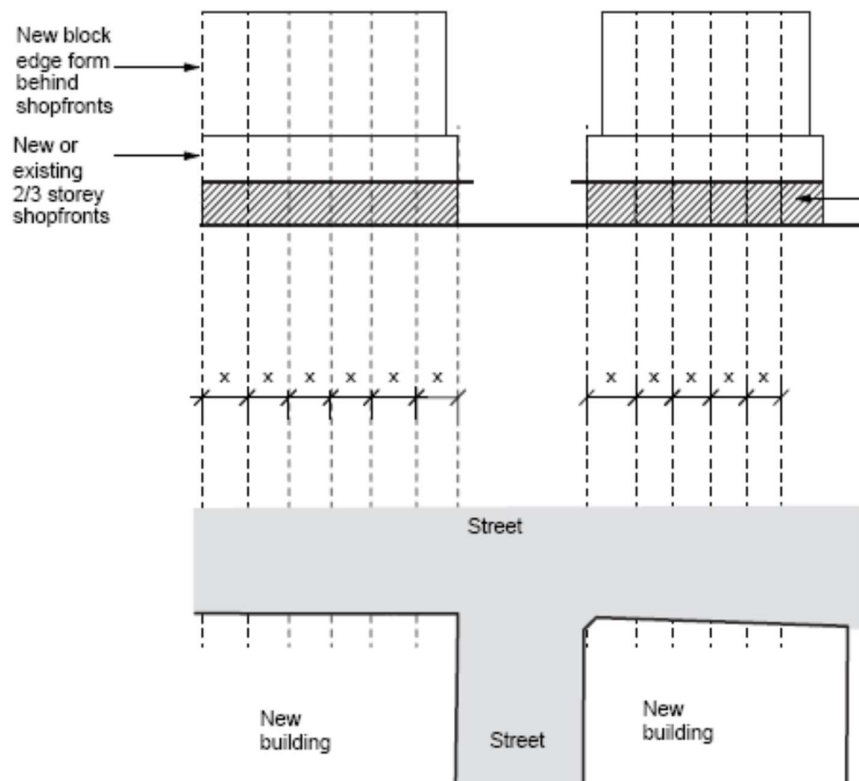
## 1.5 SUBDIVISION

### Objectives

- (a) To reinforce the expression of small lot subdivision pattern in building form.
- (b) To enrich the character and diversity within the centre.
- (c) To define the public and private domains, maintain street hierarchy, and connection.
- (d) To encourage a human scale in building design.
- (e) To encourage a diversity of shop fronts along streets.
- (f) To encourage the highest and best use of land along shopping streets.

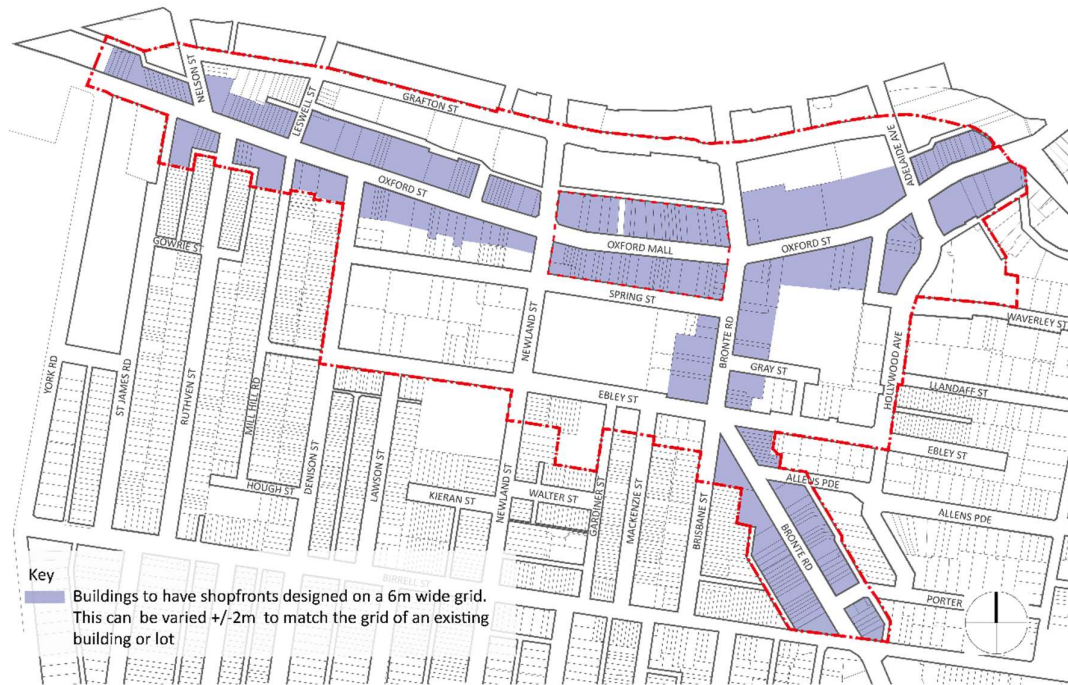
### Controls

- (a) Development is to comply with *Part B13 Subdivision*.
- (b) Development is to retain the small lot subdivision pattern that reflects the original shop fronts along streets in the Bondi Junction Centre.
- (c) Where this cannot occur due to amalgamation, the design of building elevations is to interpret the small lot subdivision pattern along the street front on lots (refer to Figure 5).
- (d) The design of building elevations on lots is to generally use a 6m grid. This 6m grid can be varied by +/- 2m in order to match an existing grid of an existing building or lot.
- (e) Comply with Figure 6 for small lot subdivision locations.



**Figure 5** Interpretation of the patterns of small lot fronts





**Figure 6** Building frontages to express the small lot subdivision pattern

## 1.6 HERITAGE AND BUILDINGS OF HISTORIC CHARACTER

### Objectives

- (a) To ensure buildings of historic character, and original shop fronts, are retained or reinterpreted.
- (b) To retain the streetscape setting of sites and buildings of historic, architectural and aesthetic significance.
- (c) To recognise the opportunities for heritage sites and contributory buildings to inform streetscape character.
- (d) To ensure developments in or adjacent to conservation areas retain and enhance the conservation values of that area.

### 1.6.1 Buildings of historic character

#### Controls

- (a) Lots identified with buildings of historic character are to retain as a minimum the facade (for a depth of 2m) of the building or preferably the whole building (refer to Figure 7).
- (b) Where a facade cannot be retained the new buildings are to interpret the scale, facade modelling including the location and percentage of glass to solid wall and the vertical and horizontal proportions of the existing building.
- (c) Where the building form, detailing or use of individual buildings of historic character have been inappropriately altered and changed, any application to upgrade or re-use the buildings must clearly demonstrate that the architectural and streetscape value of the building will be enhanced by the proposal.
- (d) Any application to demolish an identified building of historic character must clearly demonstrate that a replacement building will possess equal or higher quality contributory value regarding streetscape, character, architectural design and material quality.
- (e) New development adjacent to buildings of historic character must have facades sympathetic in vertical and horizontal proportions and alignments.
- (f) New buildings adjacent to buildings of historic character or heritage items should display proportions respectful of and build upon proportions similar to adjoining streetscape and forms.
- (g) New buildings adjacent to buildings of historic character or heritage items should draw on the predominant pattern of the streetscape. They are to be open & glazed at street level, have emphasis toward a singular enclosed building form at upper levels and be capped by a lighter more articulated element.





**Figure 7** Buildings of historic character

#### *Facades*

- (h) Original facade elements above awning level such as windows, parapets, balconies and ornamental detailing should be retained where possible.
- (i) Consistency should be achieved through; parapet height, string course both at parapet level, window proportions (sill and lintel height), awning height, fenestration and balcony elements, facade depth and modelling (refer to Figure 8).

#### *Height*

- (j) Match the streetscape proportions and scale of the heritage or contributory building facade.
- (k) The height of the building at the facade shall take into consideration existing parapets and other facade details of established surrounding development.
- (l) The height of awnings of heritage or contributory building should correspond to the surrounding area.

#### *Materials*

- (m) Ensure there is a positive integration of contemporary prefabricated building materials. Using materials similar to or compatible with the existing context (generally rendered or painted masonry).

#### *Windows*

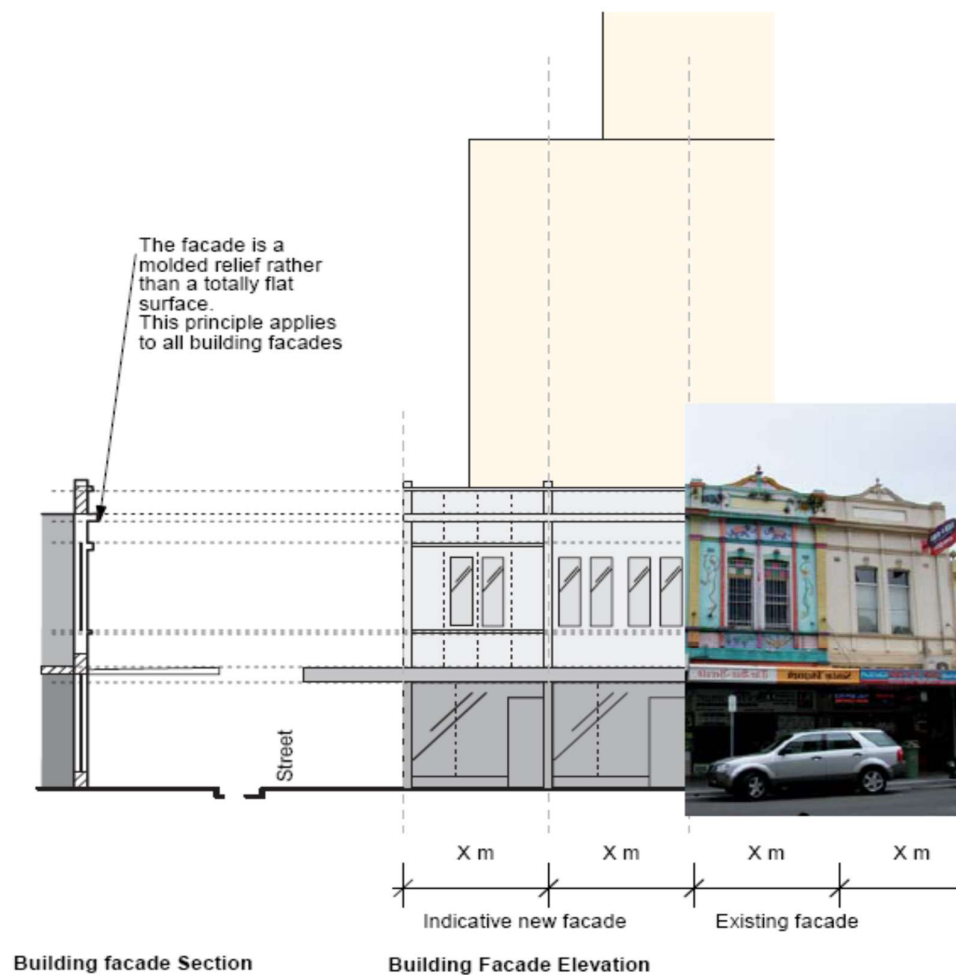
- (n) When restoring a facade that has been subject to substantial alterations over time, look to similar examples in the street to determine correct window proportion, style and materials.
- (o) Ensure the window area is proportionate to the wall mass.
- (p) Prefabricated aluminium windows will not be appropriate.

### Awnings

- (q) Existing box section awnings, cantilever, or suspended by tie rods, should be retained. New awnings should match the form of adjacent awnings and maintain the same alignment.
- (r) Pitched or domed awnings of plastic, glass or canvas construction are not permitted.

### Colour

- (s) Achieve a sympathetic juxtaposition of colour on adjacent building forms.
- (t) Ensure that a row of shops which are homogeneous or symmetrical in style adopt a uniform tonal distribution over the facade without limiting individual colour expression on each shop.



**Figure 8** Interpretation of buildings with historic character

### 1.6.2 Streets with Heritage and Buildings of Historic Character

#### Objectives

- (a) To ensure that the scale of existing height of original 2-3 storey shop fronts is retained along streets.
- (b) To enhance the streetscape setting adjacent to heritage sites.
- (c) To retain and reinforce a pedestrian scale to streets.
- (d) To encourage ongoing adaptive re-use of buildings of historic character.

#### Controls

- (a) Buildings on lots with frontages identified in Figure 9 are to have a 2-3 storey façade along the street and are to be built to the street alignment.
- (b) Balconies to the street facade are to be recessed behind the principle building facade.
- (c) Open spaces and external building forecourts at street level are discouraged on streets with heritage buildings.



Figure 9 Street Frontage in streets with heritage

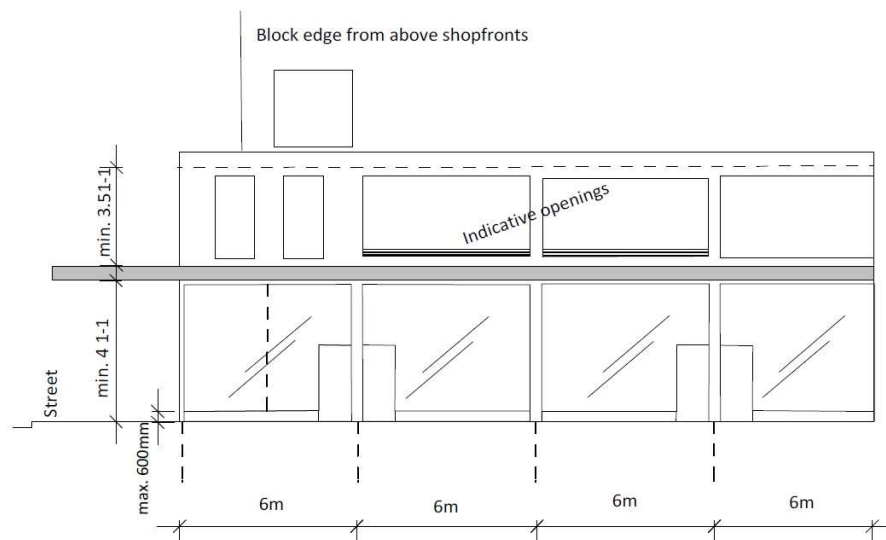
## 1.7 ACTIVE STREET FRONTAGES

### Controls

- (a) Active Street Frontages are to be provided in accordance with Figure 10, Part B16 Public Domain and the WLEP Active Street Frontages Map.



Figure 10 Location of active frontages.



New shopfronts generally 6m +/- 2m wide, small lot subdivision pattern used to design proportions of new building elevations and shopfronts in streets with contributory buildings.

Figure 11 Primary shopping street active frontage

## 1.8 BUILDING ORIENTATION

### Objectives

- (a) To ensure that new development contributes to the streetscape in a positive way.
- (b) To provide passive surveillance for the street.
- (c) To easily achieve setback distances for privacy and outlook.
- (d) To provide a frontage and clear entry facing the street.
- (e) To avoid overlooking neighbouring dwellings.
- (f) To ensure the amenity of neighbouring buildings is provided.

### Controls

- (a) Podiums are to be oriented to and address the street(s).
- (b) Orient tower forms to the front and the rear of lots where possible. Avoid orienting primary windows to side boundaries.
- (c) Blank walls are not to front public streets.
- (d) Where possible orient bathroom, laundry and other ancillary room windows to the side boundaries.

#### **Calculation rules**

*Building orientation refers to the direction that the primary windows of living rooms and external living areas face.*

*Orientation to the front means that the primary windows of living rooms and external living areas face the street and are generally parallel to the front boundary.*

*Orientation to the rear means that the primary windows of living rooms and external living areas are generally parallel to the rear boundary.*



## 1.9 STREET ALIGNMENT

### Objectives

- (a) To reinforce the character of the commercial centre through consistent setbacks.
- (b) To enhance streets as the commercial and civic space for the centre.
- (c) To provide easy and legible pedestrian access ways and entrances into buildings.
- (d) To create consistent and unified building elevations along streets.
- (e) To improve the quality of the public domain.
- (f) To ensure building facades create a human scale to the street.
- (g) To define the space of public streets and other public spaces such as squares and parks.
- (h) To maximise safety within public places.

### Controls

#### *General Controls*

- (a) Buildings are to have front elevations aligned to the street boundary with setbacks in accordance with Figures 12-14.
- (b) A continuous street frontage is to be provided.
- (c) Situations where a variation to building in alignment with the street boundary may occur includes where the building is adjacent to a heritage building that may have a curtilage, setback or important building elevation facing the side boundary. In such cases a site specific heritage response is required, or the creation of a forecourt.
- (d) Open spaces at the street front for private buildings are not permitted.

#### *Podium*

- (e) Corner sites are to be built to both street alignments.
- (f) Development in streets with heritage, identified in Figure 12, are to include a 2/3 storey street wall, with a minimum 6m setback to built form above the street wall (refer to Figure 14).
- (g) Developments on all other lots are to have front building elevations built to the street alignment to 6 storeys (refer to Figures 12 and 13).

#### *Tower*

- (h) Towers are to be setback a minimum of 6m from the street wall and parallel to the street boundary (Figures 13 and 14).

#### **Calculation rules**

- The front setback is measured from the lot boundary along the street to the outer most edge of the building elevation (not the garage or car parking area).
- Setbacks are measured at 90 degrees from the lot boundary and include any articulation to the building elevation as well as including roofed or external living areas.



Figure 12 Street alignment.

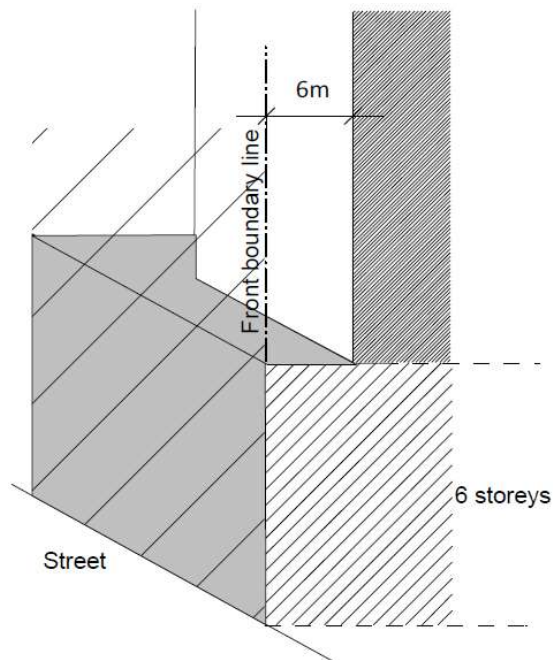
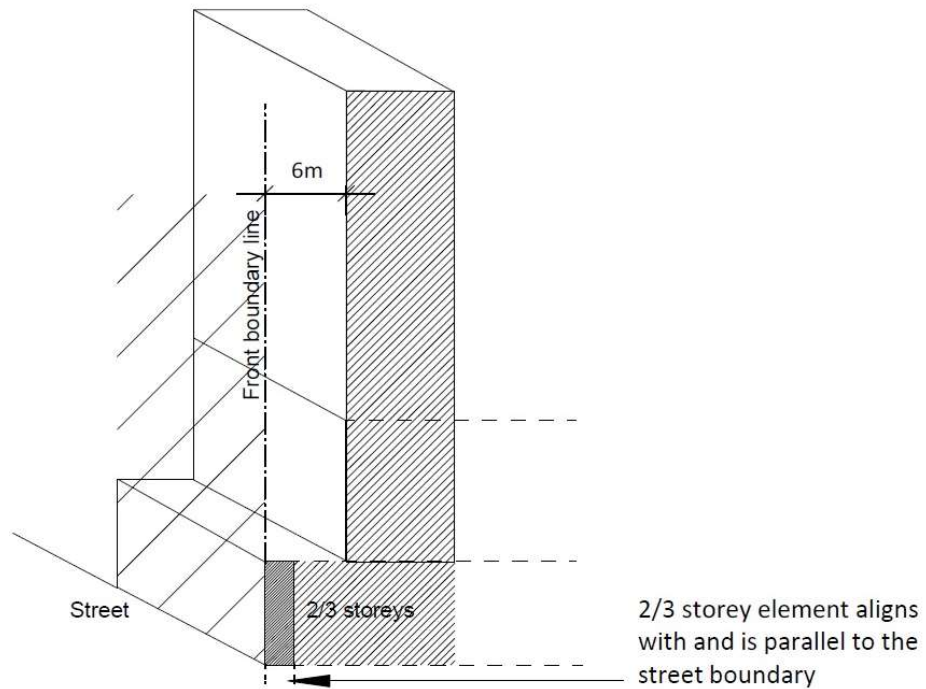
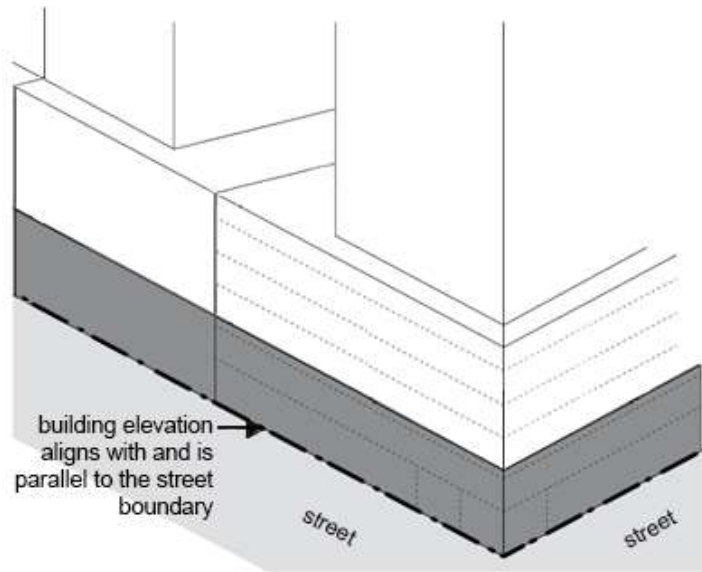


Figure 13 Setbacks from the street: buildings in street without heritage

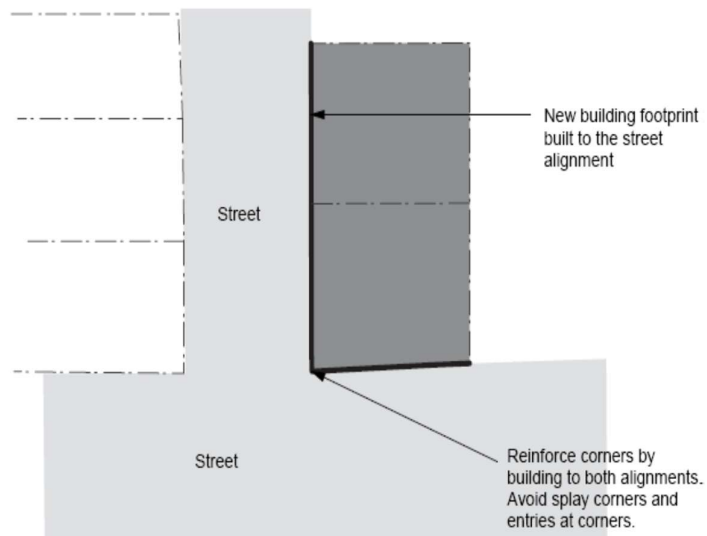


**Figure 14** Setbacks from the street – buildings in streets with heritage





**Figure 15a** Corner sites build to boundary



**Figure 15b** Corner sites build to boundary - Plan

## 1.10 SEPARATION DISTANCES

### Objectives

- (a) To provide amenity and liveability in new buildings.
- (b) To protect the amenity and liveability of existing buildings.
- (c) To facilitate visual and acoustic privacy between buildings.
- (d) To facilitate light and air to buildings.

### Controls

- (a) Development is to comply with the separation distances in Figures 16-18, and the *Apartment Design Guide* where applicable.
- (b) In podiums, windows must not be located or oriented to the side boundary for a distance of 8m from the front boundary to allow a continuous street wall to be provided.
- (c) Nil side setbacks can occur where separation distances permit, i.e. there are no windows or balconies within the relevant separation distance.

#### **Calculation Rules - Separation**

**Building orientation** refers to the direction of the external face of the building that provides the primary source of light, air and outlook to both residential uses (living room windows/doors and external living areas) and commercial uses (office or shop windows).

The measurement is to be taken from the windows/doors of the living room that give the rooms its primary source of outlook, light and air. Living areas include living rooms and external living areas such as balconies and terraces. For an external living area the measurement is taken from the outermost point of the balustrade.

**Primary windows:** For living rooms that have more than one orientation, the orientation that provides the primary source of light, air and outlook is to be used. These windows are described in the controls as primary windows.

**All other windows:** This includes bedroom windows and windows to non-habitable rooms. Living rooms that have a second orientation can also provide outlook, light and air to the room but in the case that greater privacy is required these windows/doors can be of opaque material, fixed, shaded or smaller in size.

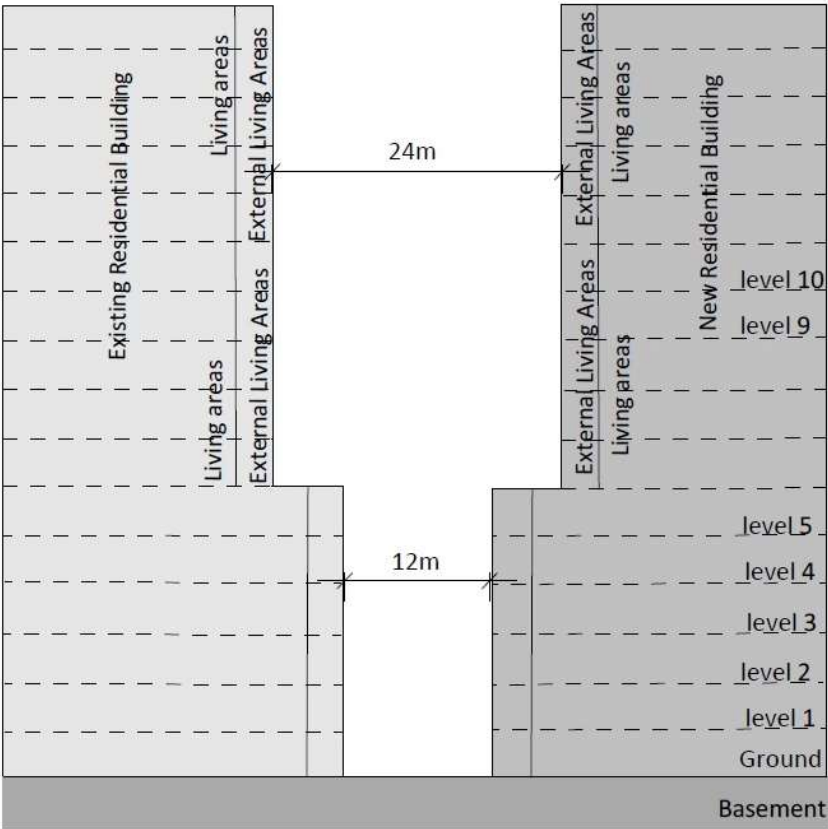


Figure 16 Minimum distances between residential living areas

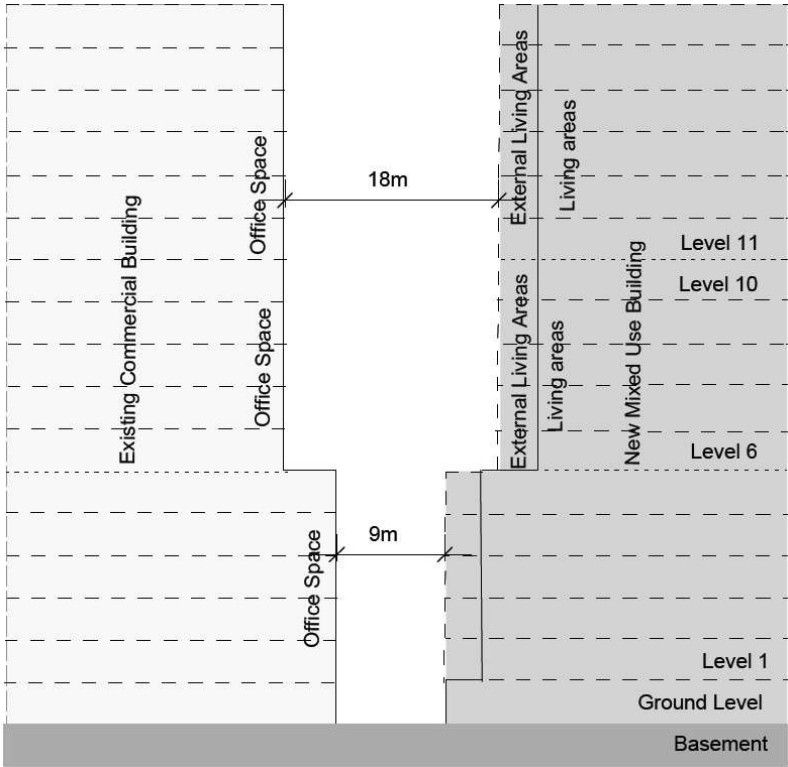


Figure 17 Separation distances between residential living areas and commercial uses

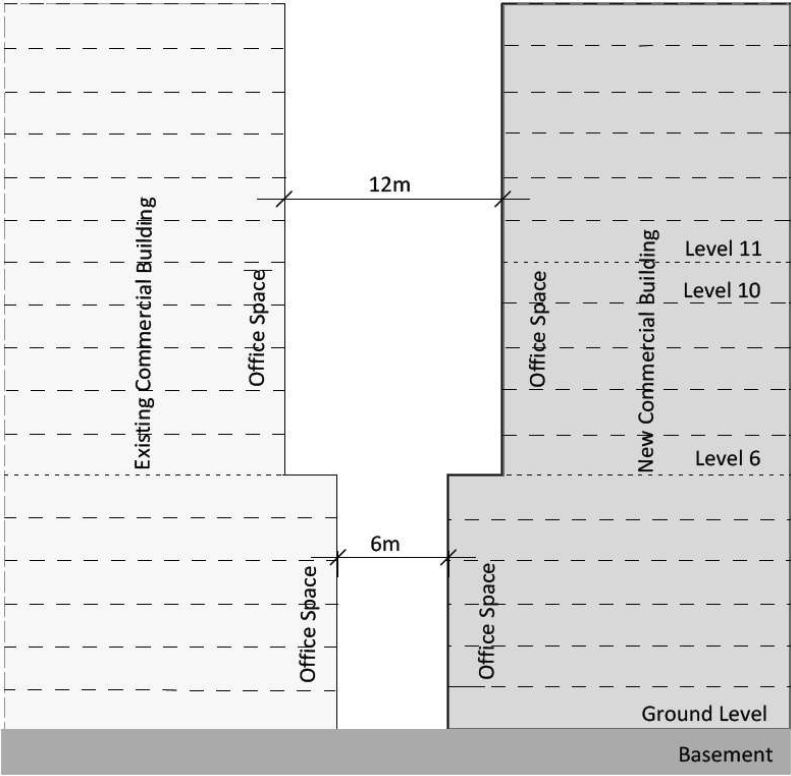


Figure 18 Separation distances between commercial uses

## 1.11 SIDE AND REAR BOUNDARY SETBACKS

### Objectives

- (a) To define the street space.
- (b) To facilitate visual and acoustic privacy between buildings.
- (c) To facilitate light and outlook.

### Controls

- (a) Refer to Figures 19, 20 and 21.

### Podium

- (b) Where a blank wall exists on the adjacent property boundary wall, a nil setback is to be provided.
- (c) The podium nil side setback must be provided for a minimum of 8m measured from a front boundary (refer to Figure 19).
- (d) Where existing neighbouring buildings have windows or balconies at podium levels facing a side boundary, the following side setback distances apply:

<b>9m min.</b>	Primary windows of living areas/balconies
<b>4.5m min.</b>	All other windows
<b>3m min.</b>	All other windows on small sites (24m wide or smaller)

- (e) On lots with rear laneways, the rear boundary setback can be nil if adequate separation distances are met. The laneway is to be included in the separation distance (refer to Figure 21).

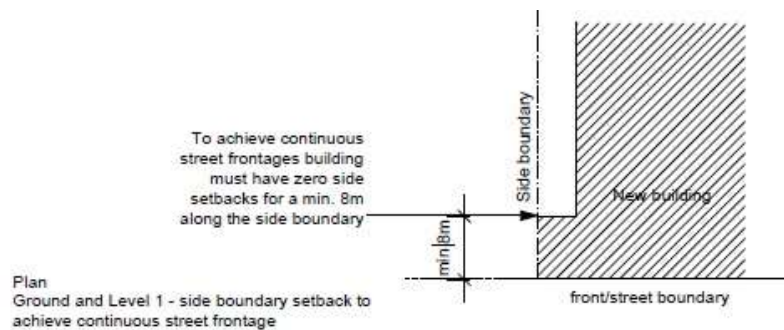
### Tower

- (f) Where existing neighbouring buildings have windows or balconies at podium levels facing a side boundary, the following side setback distances apply:

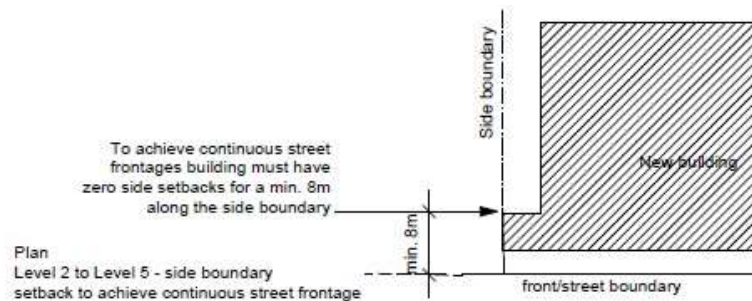
<b>12m min.</b>	Primary windows of living areas/balconies
<b>6m min.</b>	All other windows

### Calculation rules

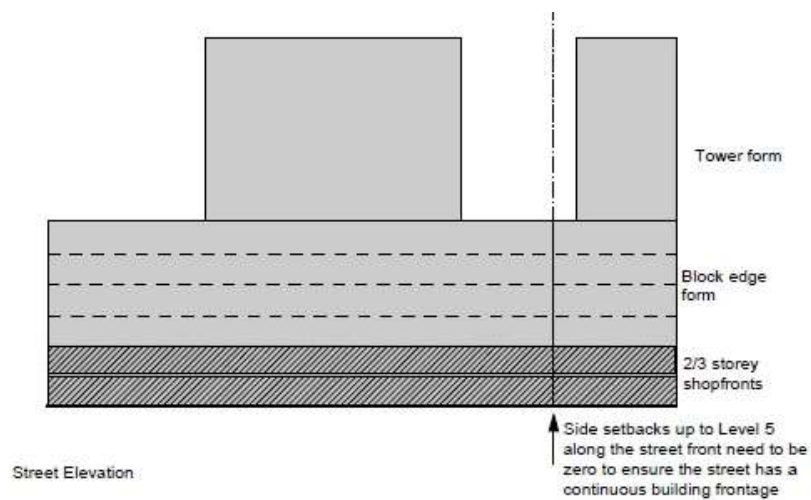
- Side setbacks are measured from the lot's side boundary to the outermost edge of the building elevation i.e. edge of balustrades to balconies, rather than the glass line.
- Setbacks are measured at 90 degrees to the lot boundary and are measured to the outer most edge of the building elevation including balconies, terraces and porches.
- Rear boundaries may be either on a laneway or where two lots back onto one another.
- Lots that extend from street to street do not have rear boundaries but rather have two street frontages.



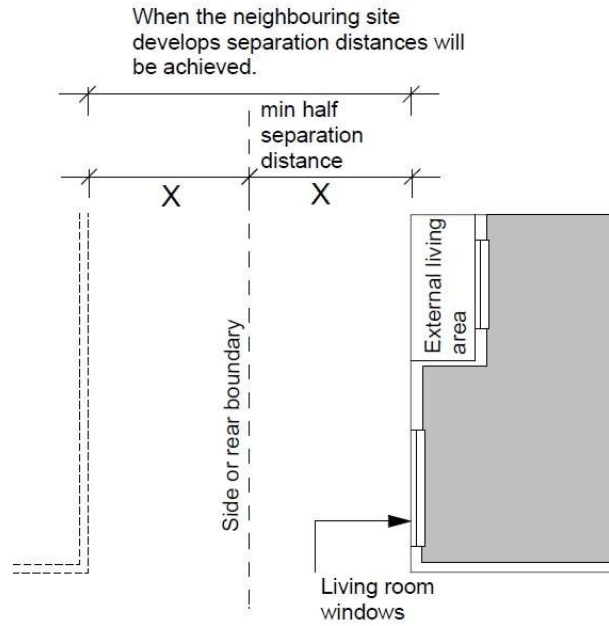
**Figure 19a** Side boundary setbacks – Ground and Level 1



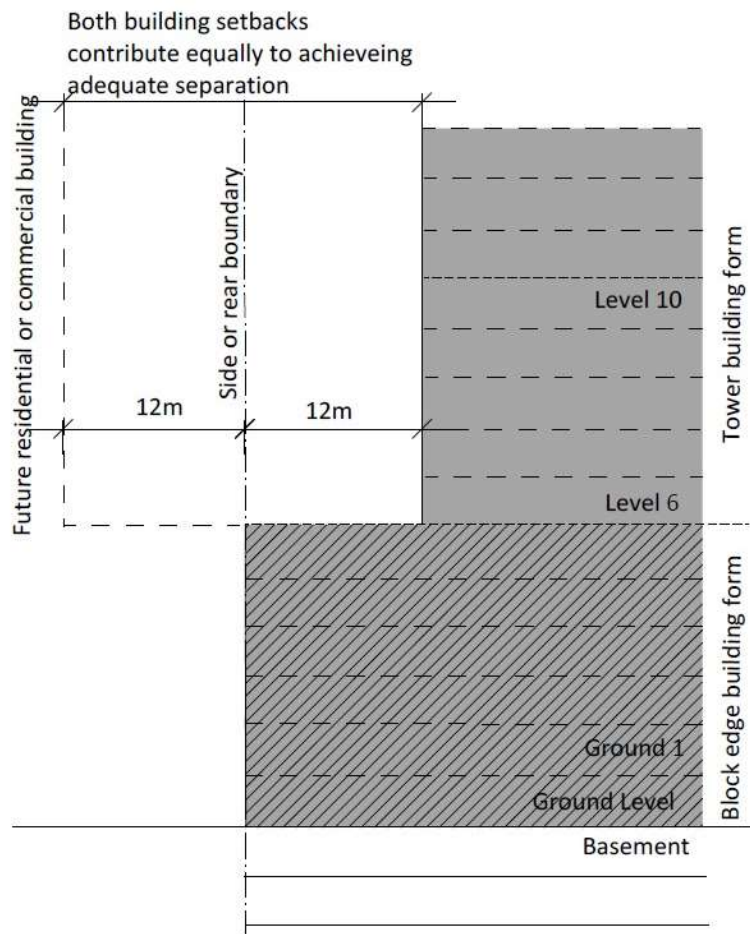
**Figure 19b** Side boundary setbacks – Level 2 to Level 5



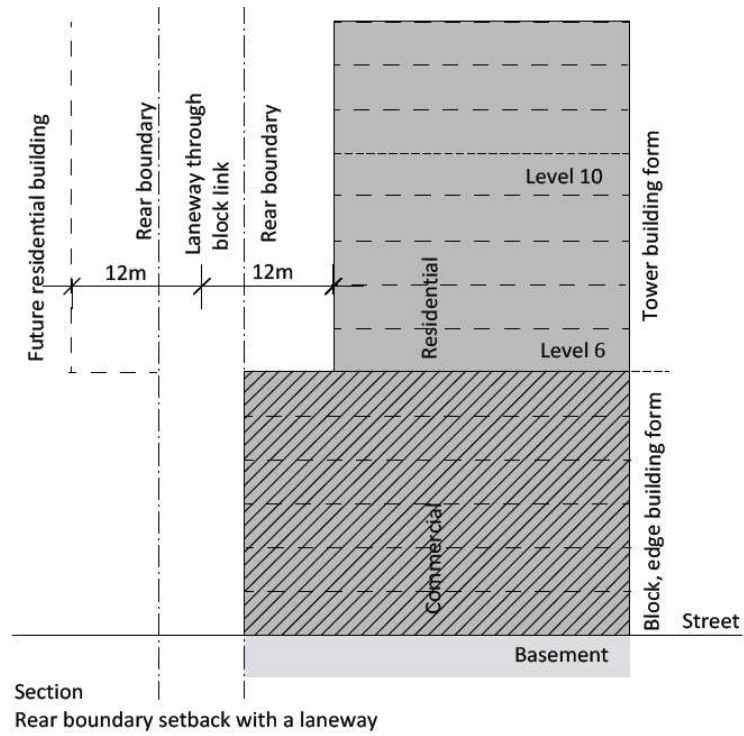
**Figure 19c** Side boundary setbacks - Elevation



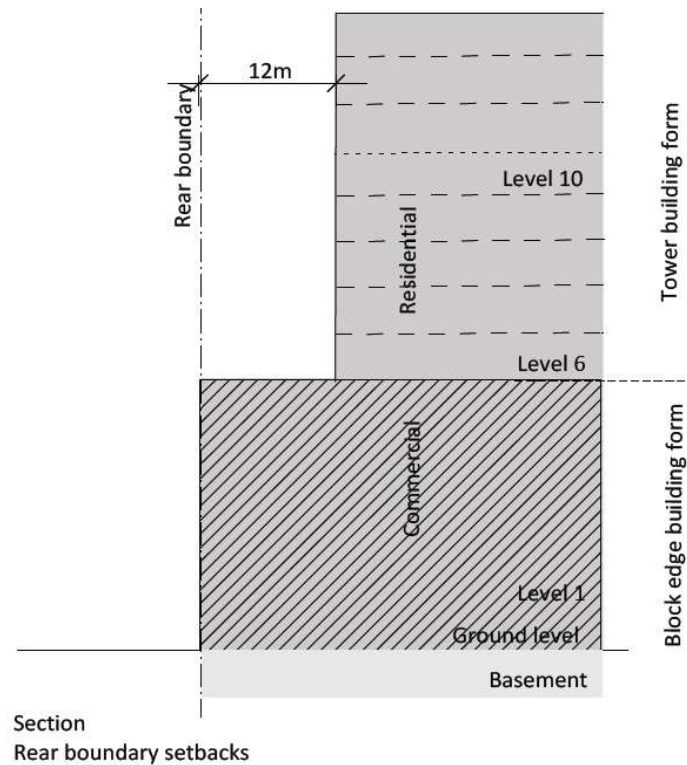
**Figure 20a** Plan - Side and rear boundary setbacks to prevent overlooking



**Figure 20 Elevation** - Side and rear boundary setbacks to prevent overlooking



**Figure 21a Section** – Rear boundary setbacks with a laneway



**Figure 21b Section** - Rear boundary setbacks



## 1.12 BUILDING FOOTPRINT

### Objectives

- (a) To reinforce the street edge.
- (b) To provide amenity in terms of solar access and natural ventilation.
- (c) To promote sustainable design that is less reliant on artificial heating, cooling and lighting by encouraging thin cross section buildings.
- (d) To provide ground and first floor plates which cater for commercial uses and to encourage commercial uses within the whole podium.
- (e) To provide for flexible commercial or residential uses in the tower components of buildings.
- (f) To ensure that shop fronts line commercial shopping streets.

### Controls

#### *General Controls*

- (a) To achieve narrow cross section buildings consider using atria, light wells and courtyards open to the sky to achieve additional daylight and or stack and cross ventilation.
- (b) The use of skylights to provide the primary source of daylight and ventilation to habitable rooms is not permitted.
- (c) Provide common areas such as corridors and entrances with natural light and cross ventilation i.e. openable windows.
- (d) Maximise daylight to all areas such as lobbies, corridors, kitchens and bathrooms by limiting the depth of buildings.
- (e) Avoid or minimise the reliance on mechanical ventilation or air conditioning to these areas.

#### *Podium*

- (f) Commercial:
  - (i) Commercial uses are to be provided in podium floors.
  - (ii) Podium floor plates may have a maximum 100% site coverage provided setback and separation controls are met (refer to Figure 22).

#### *Tower*

- (i) Commercial:
  - (i) Tower building forms are to be designed so that no habitable space is more than 15m from a source of daylight (refer to Figure 23).
- (j) Residential:
  - (i) Residential tower buildings are to comply with the *Apartment Design Guide*.

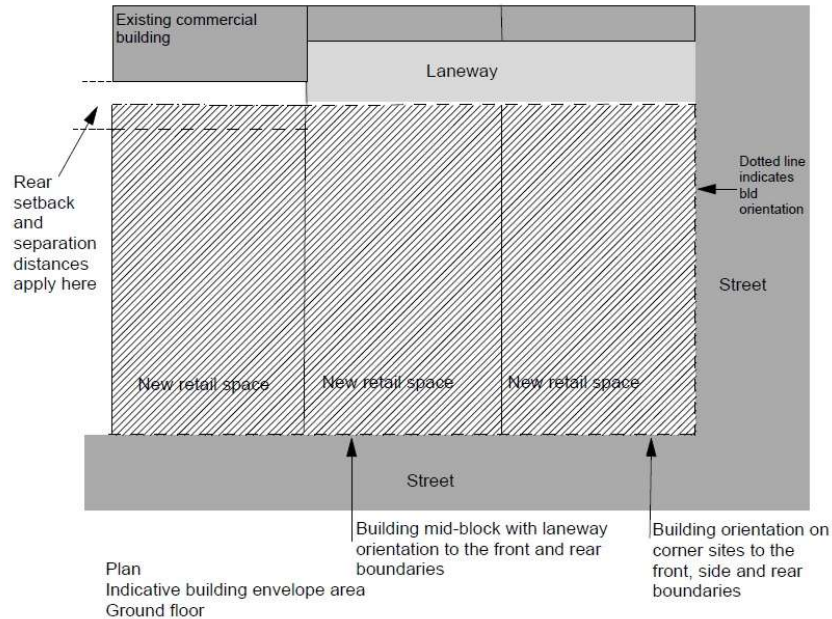
#### **Calculation rules**

**Building depth** refers to the dimension measured from the buildings front or street elevation to the rear elevation. Building depth includes the internal plan depth of the dwelling; it does not include external living areas.

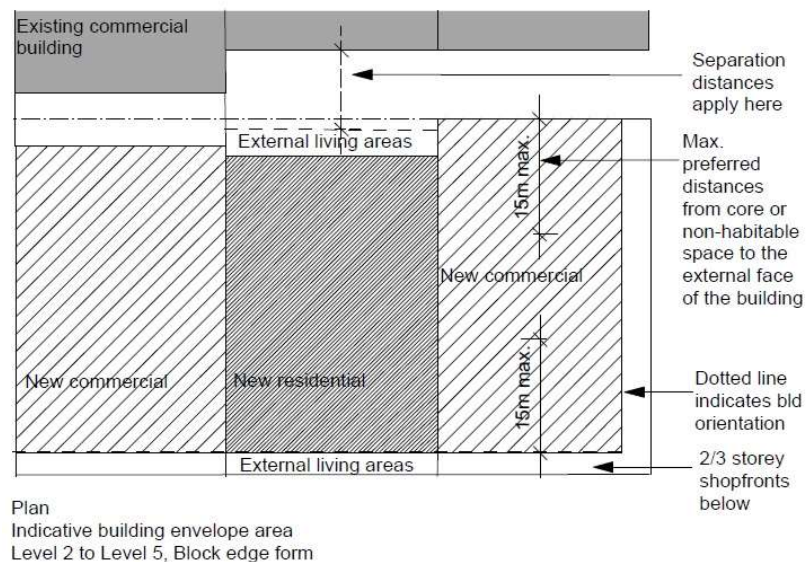
**Building width** is measured from side building elevation to side building elevation. Building width is set by the width of the site minus the required side setback.

*Mixed-use buildings may have a deeper ground level footprint to accommodate commercial uses with a narrower residential footprint above.*

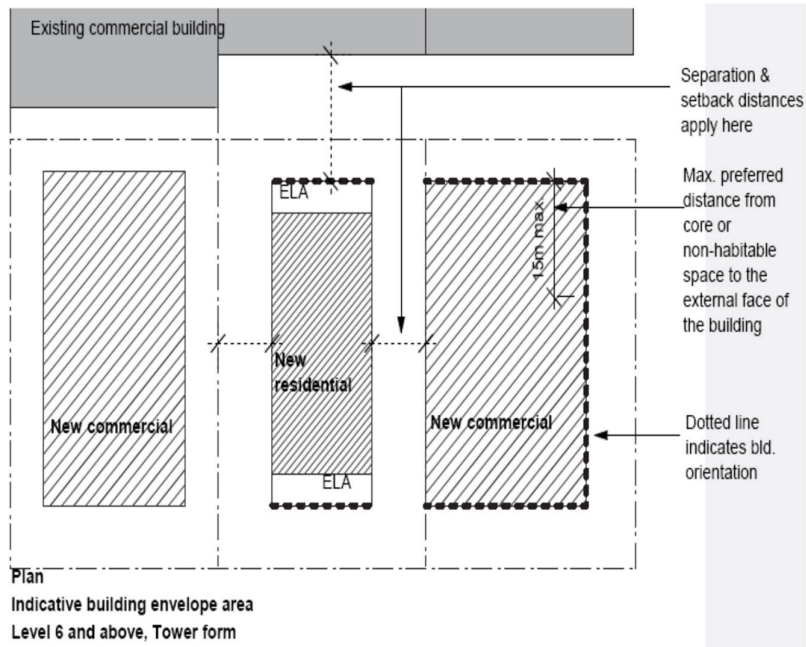
*Some sites may have irregular site conditions such as topography or site shape. Such sites may require particular footprint design solutions that address such irregularities. For example buildings on narrow sites may require slender footprints to protect the amenity of neighbouring sites and to achieve the required setbacks. Sites on steep topography may require detached building footprints to account for the differences in grade.*



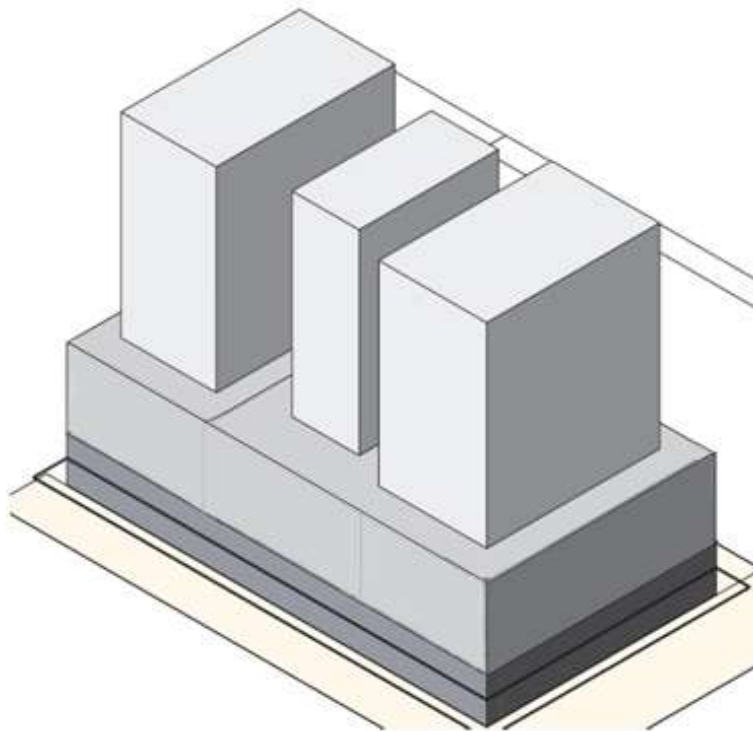
**Figure 22a** Indicative building footprints on a small site: Ground and Level 1



**Figure 22** Indicative building footprints on a small site: Levels 2 - 5



**Figure 23** Indicative building footprint on a small site: tower building form



**Figure 24** Indicative block 3D modelling of three footprint components

## 1.13 NUMBER OF STOREYS

### Objectives

- To ensure that internal amenity is not compromised to maximise development potential.
- To ensure adequate internal amenity is provided through appropriate floor to ceiling heights.
- To ensure buildings create a human scale to the street.
- To encourage development and redevelopment potential.
- To provide a transitional scale between commercial and residential.
- To strengthen the urban form with consistent heights along streets.

### Controls

- Refer to the WLEP Maximum Height of Buildings map for absolute building heights.
- Development may not achieve the maximum numeric heights due to topography or other site conditions.
- Development must not exceed the maximum number of storeys for the site as specified on Figure 25.
- Lots are to ensure they do not overshadow neighbouring or adjacent residential lots so as to preserve solar access to private open space.
- Lots to the southern side of Ebley Street, and the southern side of Bronte Road are to drop to 2 storeys at the rear to achieve solar access for adjacent properties.



Figure 25 Maximum number of storeys



## 1.14 VIEWS, VISTAS AND TREE PRESERVATION

Bondi Junction is located on a ridge which provides Bondi Junction with vistas of the Woollahra ridge slopes and Harbour Foreshore glimpses to the north down Newland Street and Bronte Road. To the south, there are vistas of the suburbs of Queens Park, Randwick and Clovelly.

The most important views within Bondi Junction occur along its streets. These views along streets frame the overall visual quality of the centre and help to define and differentiate different places within the centre. As such they are intrinsic to the quality of the urban environment and are to be retained and enhanced in the future.

### Objectives

- (a) To retain significant vistas.
- (b) To recognise the importance of Bondi Junction Centre or street views.
- (c) To enhance views and vistas throughout the centre.
- (d) To retain significant trees and vegetation.

### Controls

- (a) Retain vistas along Newland Street, Bronte Road and Grosvenor Street both to the south and the north.
- (b) No building or structure is to build into or on a street view corridor.
- (c) Development proposals that open up significant vistas from the public domain are encouraged, particularly north-south vistas.
- (d) Comply with Figure 26 for locations of views and vistas referred within this section.



Figure 26 Views and vistas

**1.15 OPEN SPACES AT THE STREET FRONTAGE****Objectives**

- (a) To ensure private buildings are built to the street alignment.
- (b) To retain a consistent alignment along streets.
- (c) To retain and increase activity on the street front.
- (d) To increase safety of the public domain and passive surveillance.
- (e) To encourage public buildings to create forecourts that are well designed and enhance the public domain.

**Controls**

- (a) New open spaces on the street front for private buildings are not suitable for Bondi Junction.
- (b) New open spaces on the street front for public buildings may be considered if they meet the following controls:
  - (i) New open spaces require active frontages along all the built sides of the space.
  - (ii) Logical and functional pedestrian connections through and beyond the space are to be provided.
  - (iii) Clear sight lines into and throughout the space.
  - (iv) The space must be accessible and useable to the public.
  - (v) Public open spaces must not be located on block and street corners, and must be a min. of 10m from a corner.

**1.16 BUILDING ELEVATIONS****Objectives**

- (a) To establish a building's identity in the streetscape and contribute to the centre as a whole.
- (b) To ensure elevations reflect the use of the building, and address environmental conditions.
- (c) To promote high architectural quality in buildings.
- (d) To create buildings which respond to environmental conditions.
- (e) To reduce reliance on mechanical heating and cooling.
- (f) To improve visual quality of communal spaces and public places.

**Controls**

- (a) All elevations must be architecturally designed and contribute to the character of the street in which they are located.
- (b) Design building elevations which incorporate the principles of passive design and the properties of thermal mass, glazing and insulation, to reduce the need to artificially heat or cool.
- (c) Provide openable windows to living and working environments.
- (d) Facades are not to be totally flat but rather to have relief modelling.
- (e) Refer to Figures 27 and 28 for indicative elevations and facades.

*First Floor elevation*

- (f) Elevations are to be composed of a solid wall with punched openings. The solid wall is to have relief modelling.
- (g) The horizontal proportions of the facade must relate to the ground level shop fronts.
- (h) Facades can have an openness ratio of up to 35% of one bay of a facade, the remaining 65% must be solid.

*Second to fifth floor elevation*

- (i) Elevations are to be composed of a solid wall with punched openings.
- (j) The horizontal proportions of the facade must relate to the ground level shop fronts.
- (k) Building facades can have an openness ratio of up to 45% of one bay of a facade, the remaining 55% must be solid.
- (l) Balconies must be recessed from the main elevation.
- (m) The tower component of buildings can be highly individual in character.

*Above fifth floor elevation*

- (n) The maximum unarticulated wall length is 25m<sup>2</sup>.
- (o) Use solar protection elements appropriate to north facing facades such as awnings, deep reveals, roof overhangs.
- (p) Use solar protection elements appropriate to east or west facing facades such as external louvers, shutters, screens. These may be used in conjunction with awnings, deep reveals, roof overhangs.

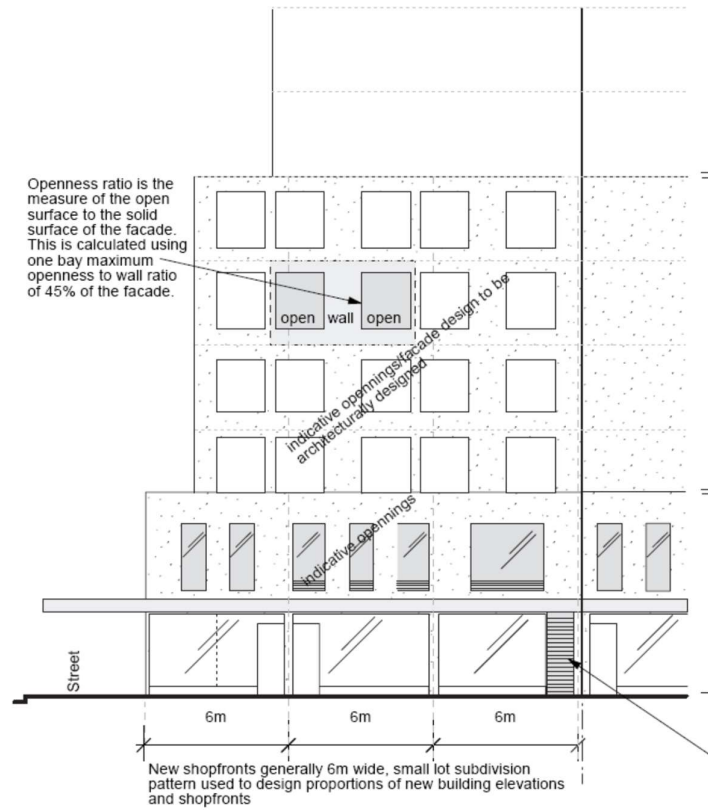


Figure 27 Elevations

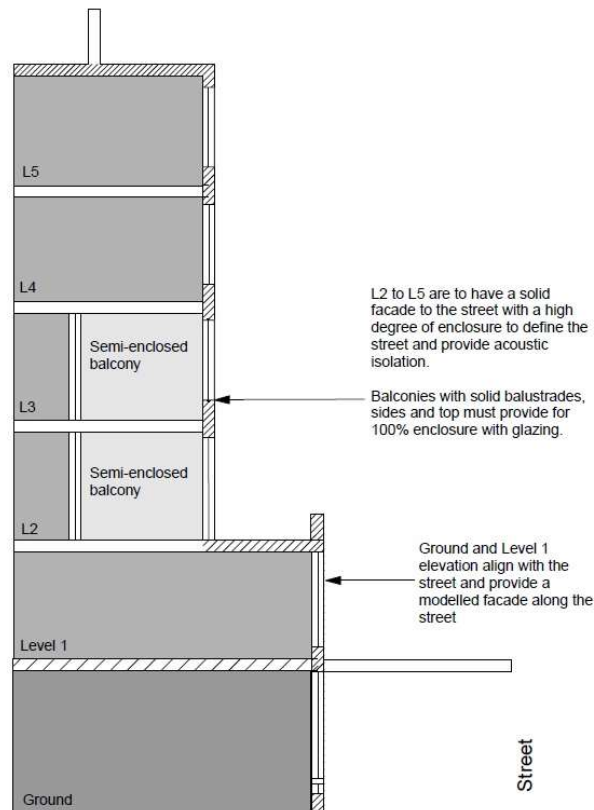


Figure 28 Façade section



**1.17 DESIGNING BUILDINGS FOR FLEXIBILITY****Objectives**

- (a) To improve the quality of the built environment and apply sustainable practices.
- (b) To encourage the design of low energy consumption, durable, flexible, adaptable buildings.
- (c) To promote the design of robust buildings to allow flexibility over time, for conversion between residential and commercial uses.

**Controls**

- (a) Design the podium component of buildings (Level 2 – Level 5) to permit adaptation for other future uses, with minimal structural and service alteration by:
  - (i) Concentrating the location of service elements such as fire stairs, air conditioning units, service risers, toilets, kitchens and the like together to allow larger free floor plate areas; and
  - (ii) Designing service areas and risers generously to make them readily accessible and capable of additional capacity.

**1.18 CEILING HEIGHTS****Objectives**

- (a) To ensure internal amenity is maximized.
- (b) To promote solar access into all buildings.
- (c) To ensure adequate spatial provision for services.

**Controls**

- (a) Coordinate internal ceiling heights and slab levels with external height datum lines such as datum and parapet lines set by surrounding existing buildings.
- (b) Increase the sense of space in rooms through provision of well-proportioned rooms.
- (c) Use tall windows or highlight windows, as well as light shelves and fan lights to reflect natural light deeper into a floorplate.
- (d) Stack wet areas from floor to floor to allow taller floor to ceiling heights in habitable areas.
- (e) Commercial and retail buildings must provide the following ceiling heights:
  - (i) Minimum 4m floor to ceiling heights at Ground Floor
  - (ii) Minimum 3.3m floor to ceiling heights at First Floor and above.

**1.19 EXTERNAL LIVING AREAS****Objectives**

- (a) To provide an external living area for each dwelling.
- (b) To enhance the amenity of internal living spaces.
- (c) To ensure that external living areas do not adversely impact on the amenity of nearby properties.

**Controls**

- (a) External living area is to be screened to achieve visual privacy if located less than 4m from a side boundary.
- (b) The rooftops of developments can be used to provide external living areas.
- (c) Detail and design balconies or terraces in response to the local climate and context, thereby increasing their usefulness. This may be achieved by:
  - (i) Locating balconies and terraces facing predominantly north or east, utilising sun screens, shutters and operable walls to control light and wind;
  - (ii) Providing balconies or terraces with operable screens, Juliet balconies or operable walls/sliding doors with a balustrade may be preferable in special locations where noise or high winds prohibit other solutions;
  - (iii) On towers, use cantilever balconies, partially cantilever balconies and/or recessed balconies in response to daylight, wind, acoustic & visual privacy;
  - (iv) Design balustrades to allow views and casual surveillance of the street while providing for safety and visual privacy; and
  - (v) Detail balustrades using a proportion of solid to transparent materials to address sight lines from the street, public domain or adjacent development.

## 1.20 WIND MITIGATION

### Objectives

- (a) To mitigate the effects of strong wind at street level.
- (b) To ensure wind does not preclude the functioning of the centre's key uses.
- (c) To encourage development to utilise the predominant breeze direction to inform an energy efficient design.

### Controls

- (a) Buildings shall not create uncomfortable or unsafe wind conditions in the public domain that exceed the Acceptable Criteria for Environmental Wind Conditions.
- (b) Locate or design outdoor areas to ensure places with high wind level are avoided.
- (c) All applications for buildings over 5 storeys in height shall be accompanied with a wind environment statement, unless a wind tunnel study is required. For buildings over 9 storeys and for any other building which may be considered an exposed building, applications shall be accompanied by a wind tunnel study report (refer to *Annexure E1-1*).
- (d) Acceptable Criteria for Environmental Wind Conditions:

Area Classification	Limiting Weekly Maximum Gust-Equivalent Mean	Limiting Annual Maximum Gust
Outdoor dining areas, amphitheatres etc	3.5m/s	10 to 13m/s
Main retail centres such as Oxford St Mall, parks, communal recreational areas such as common swimming pool on the podium	5.5m/s	13m/s
Footpaths and other pedestrian access ways	7.5m/s	16m/s
Infrequently used laneways, easements, private balconies	10m/s	23m/s

**Note:** Gust-Equivalent Mean is defined as the maximum 3 second gust divided by a local Gust Factor for the local wind speed. It is recommended that the local gust factor be derived from the measured local turbulence intensity. If the mean wind speed happens to be greater than the Gust-Equivalent Mean then the Mean wind speed is to be adopted in place of the Gust-Equivalent Mean.

The Annual Maximum Gust wind speed criteria can be used as an alternative to the Gust-Equivalent Mean Criteria. If the Gust-Equivalent Mean criteria are being used then a check should also be made to ensure that all areas studied are within the Annual Maximum Gust wind speed of 23m/s.

When assessing the impact of a proposed development, no increase over the existing wind conditions is acceptable unless the increase over the existing conditions is such that the relevant criterion for that type of space is still satisfied.

**Calculation rules**

*Natural wind conditions are intensified by certain types of buildings by the way they relate to the surrounding area. In this section, those buildings are called exposed buildings.*

*A building may be considered exposed if half or more of its height rises above surrounding buildings and/or the building lies on the perimeter of a built up area.*

*Exposed buildings are likely to create unpleasant and even dangerous high winds, mainly in three locations: at the base, around corners or through arcades or other openings at the base of the building.*

*In addition the areas within the exposed buildings that could potentially experience adverse wind effects are the areas on the podium, terraces on the roof or on setbacks in the tower as well as projecting or corner balconies.*

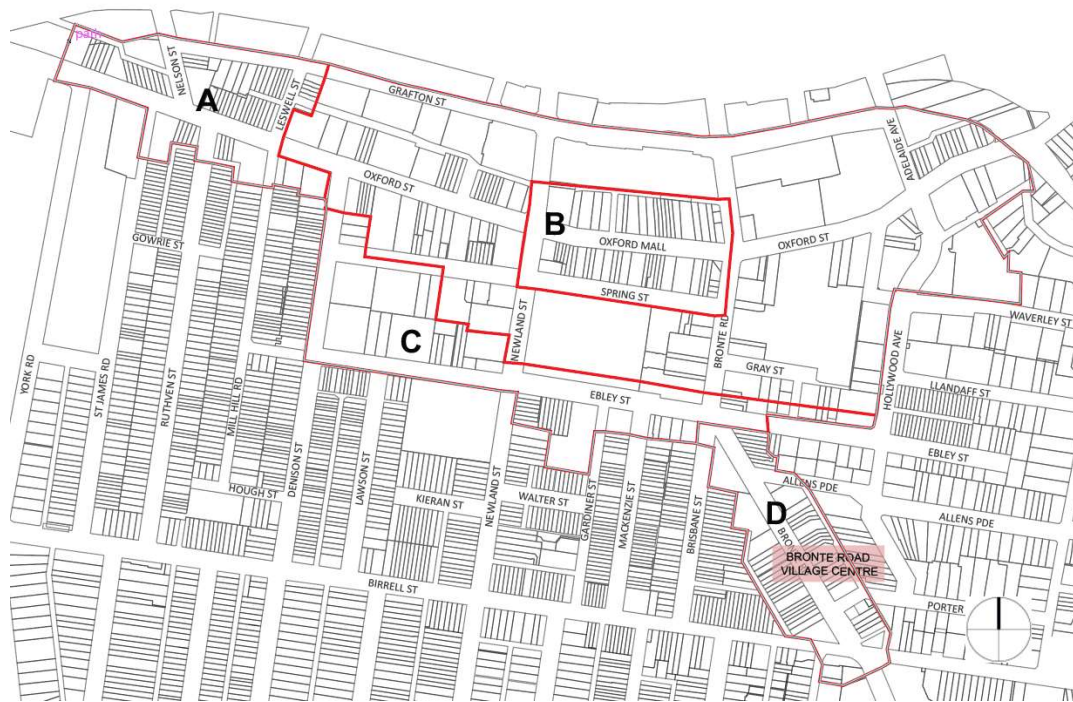
## 1.21 CHARACTER AREAS

Bondi Junction contains a number of areas that contain similar characteristics and development potential, and are known as Character Areas as shown in Figure 29.

These include:

- A – West Oxford Street
- B – Oxford Street Mall
- C – Ebley Street Transition Corridor
- D – Bronte Road Village Centre

The additional provisions provided in Part 1.21 and 1.22 apply to these Areas.



**Figure 29** Neighbourhood Areas

### Objectives

- (a) To ensure that development is consistent with the desired future character of the Bondi Junction centre.

### Controls

- (a) Development within the Bondi Junction centre must be consistent with the desired future character objectives for that area.



**1.21.1 West Oxford Street****Figure 30 West Oxford Street Area****Existing Character and Built Form**

West Oxford Street has a “village”-feel character with low-density built-form. The majority of existing development contains ground floor awnings and minimal front setbacks. Little development exceeds two storeys in scale.

The Area contains a mix of both large and small lots, providing opportunity for a mix of retail and commercial formats.

**Desired Future Character Objectives**

- (a) To protect the residential amenity of surrounding residential sites.
- (b) To retain established low-density character and “village” feel.

1.21.2 Oxford Street Mall

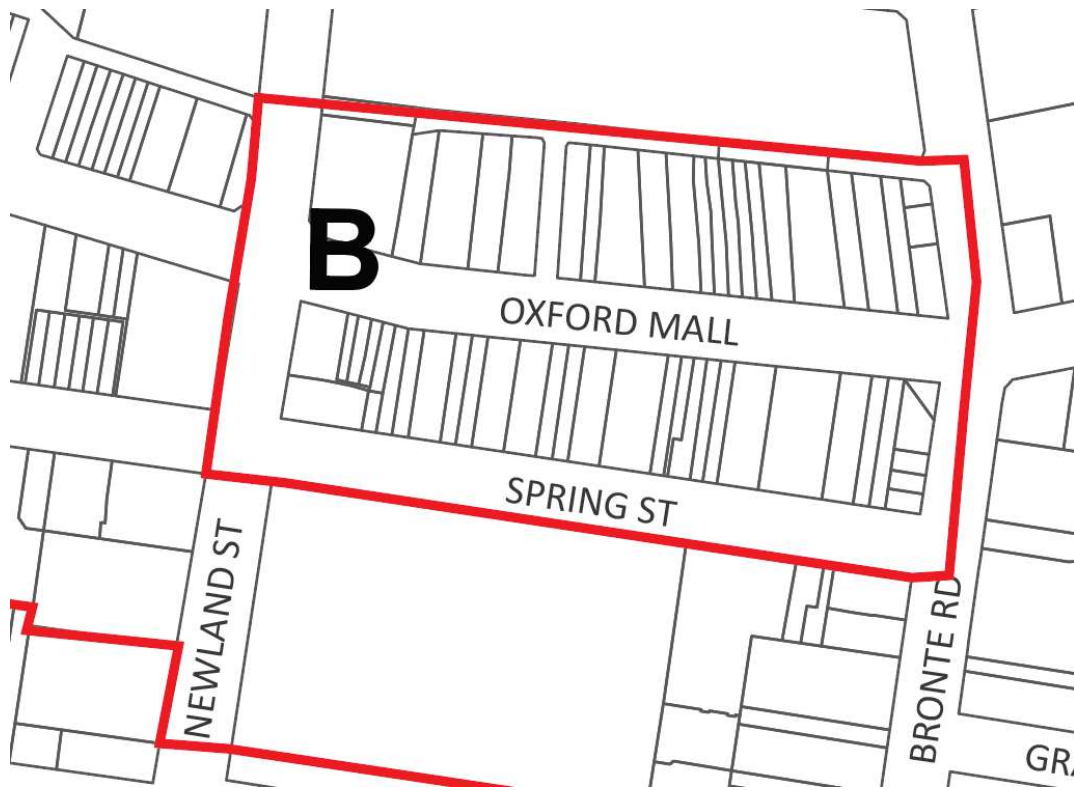
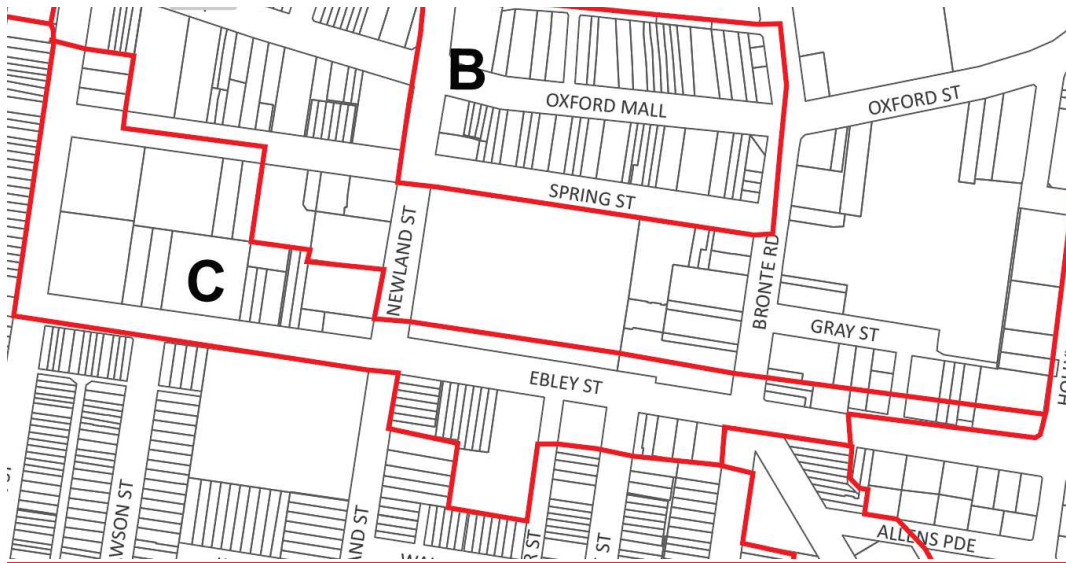


Figure 31 Oxford Street Mall Area

Refer to Part 1.22 of this DCP.

**1.21.3 Ebley Street Transition Corridor****Figure 32 Ebley Street Transition Corridor Area****Existing Character and Built Form**

Larger format non-residential land uses, including civic buildings and retail are provided within this area, facilitated by the larger lot sizes.

**Desired Future Character Objectives**

- (a) To facilitate larger format non-residential land uses.
- (b) To provide for a visual transition between the higher density development to its north, and the lower density development to its south.

1.21.4 Bronte Road Village Centre

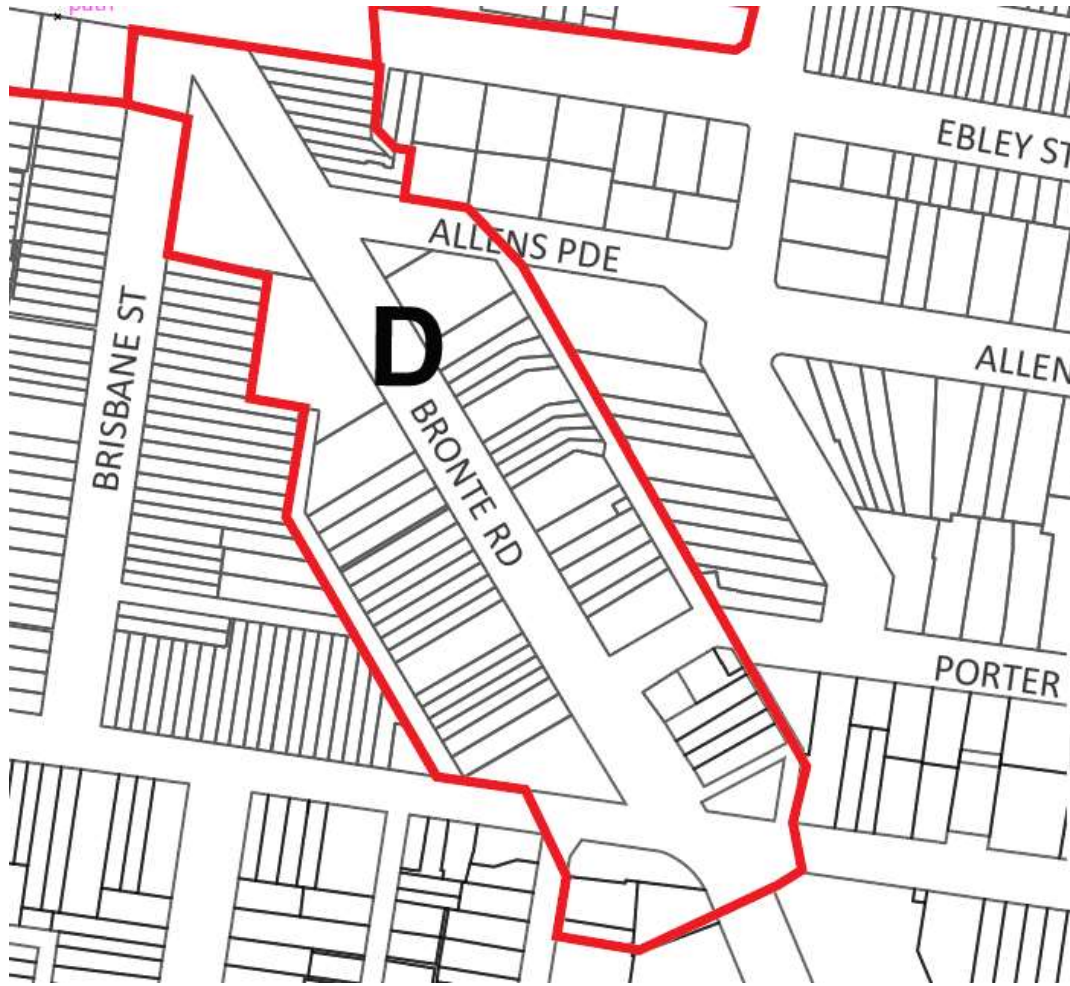


Figure 33 Bronte Road Village Centre Area

Refer to Part 3.1.1 of this DCP.

## 1.22 OXFORD STREET MALL PROVISIONS

### 1.22.1 Building to street alignment and street setbacks

#### Objectives

- (a) To provide street edges that reinforce, improve or support the hierarchy and character of specific streets.
- (b) To establish desirable spatial proportions within the street and definition of street edge.
- (c) To create a clear transition between public and private space.
- (d) To locate active uses, such as shop fronts, closer to pedestrian activity areas.
- (e) To assist in achieving visual privacy to apartments from the street.
- (f) To create good quality entry spaces to lobbies, foyers or individual dwelling entrances.
- (g) To allow an outlook to, and surveillance of, the street.
- (h) To maintain sun access to the public domain.

#### Controls

- (a) Buildings in Oxford Street Mall are to be built to the street alignment as set out in Figure 3429.
- (b) Minor projections into front building lines and setbacks for sun shading devices, entry awnings and cornices are permissible.

### 1.22.2 Street Frontage Heights

#### Objectives

- (a) To strengthen the urban form of Oxford Street Mall with consistent street wall heights.
- (b) To achieve comfortable street environments for pedestrians in terms of daylight, scale, sense of enclosure and wind mitigation as well as a healthy environment for street trees.
- (c) To enhance the distinctive character of streets within Bondi Junction Centre.
- (d) To protect solar access to key streets and public spaces.

#### Controls

- (a) Buildings must comply with the relevant street frontage heights as shown in Figures 3429 - 372
- (b) All new buildings and additions or alterations to existing buildings on the north side of Oxford Street Mall must comply with the sun access plane illustrated in Figures 359 and 361, irrespective of the existing height of nearby buildings.
- (c) The erection of a building so that any part of the building is above the envelope specified in the relevant sun access diagram is not permitted, unless that part of the building is a minor architectural roof feature.

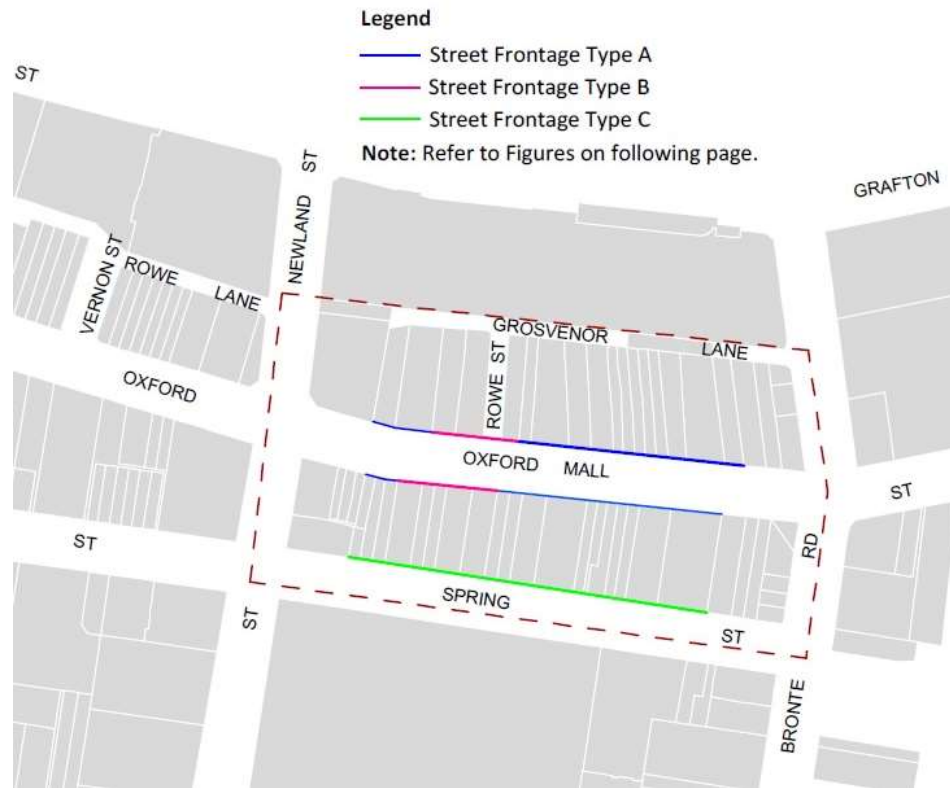


Figure 3429 Street frontage heights

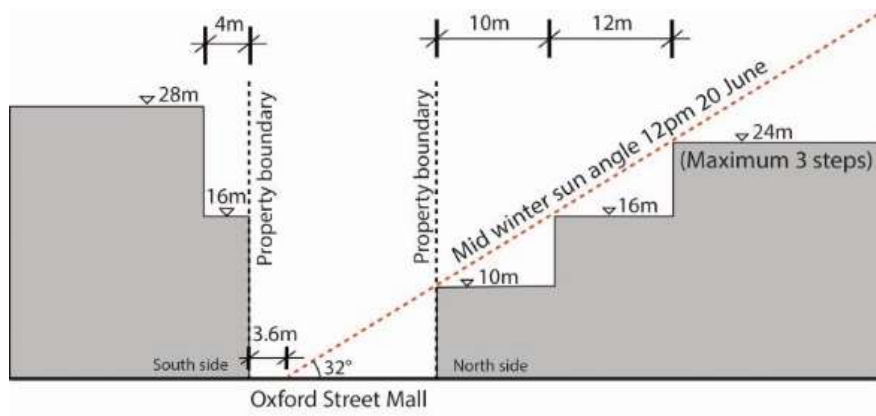


Figure 350 Street frontage Type A – Oxford Street Mall



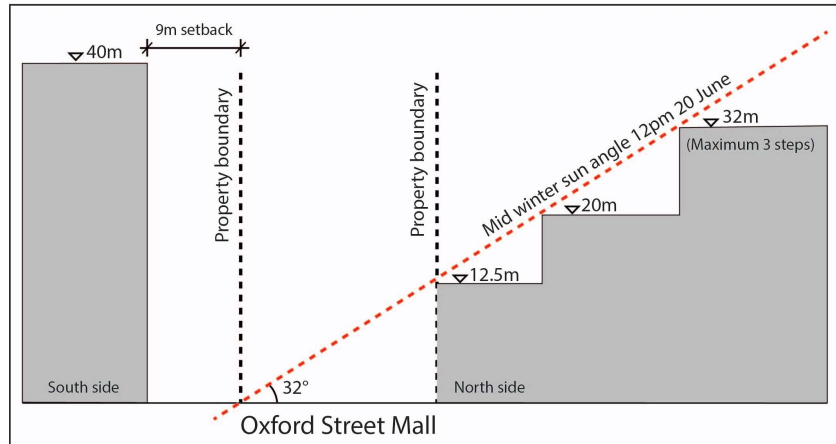


Figure 361 Street frontage Type B – Rowe Street

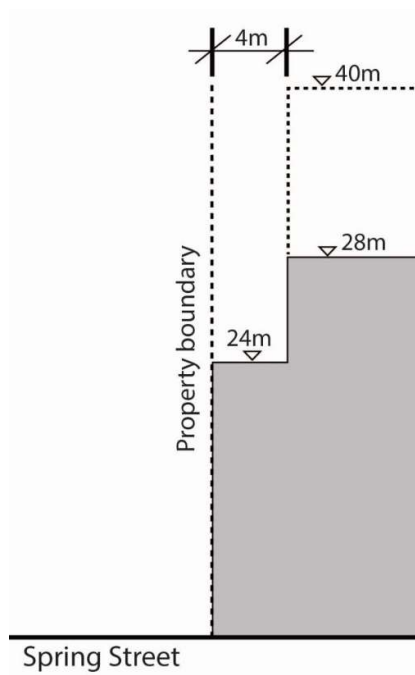


Figure 372 Street frontage Type C – Street frontage

**1.22.3 Building Depth and Bulk****Objectives**

- (a) To promote the design and development of sustainable buildings.
- (b) To promote living and working environments with good internal amenity and minimise the need for artificial heating, cooling and lighting.
- (c) To provide viable and useable commercial floor space.
- (d) To achieve usable and pleasant streets and public domain at ground level by controlling the size of upper level floor plates of buildings.
- (e) To allow for view sharing and view corridors.
- (f) To reduce the apparent bulk and scale of buildings by breaking up expanses of building wall with modulation of form and articulation of facades.

**Controls**

- (a) On land zoned B3 Commercial Core, above street frontage height the preferred maximum floor plate area of a building is 1000m<sup>2</sup> GFA.
- (b) All points on an office floor should be no more than 10m from a source of daylight (e.g. window, atria, or light wells). The preferred depth for office floors with openings on one side is 10m. The preferred depth for office floors with openings on two opposite sides is 20m.
- (c) Use atria, light wells and courtyards to improve internal building amenity and achieve cross ventilation and/or stack effect ventilation.

**1.22.4 Pedestrian Amenity****Objectives**

- (a) To improve access in Oxford Street Mall area by providing new through site links and enhancing existing links as redevelopment occurs.
- (b) To ensure through site links have active frontages along their length where possible.
- (c) To provide for pedestrian amenity and safety.
- (d) To retain and further develop laneways and small spaces as useful and interesting pedestrian connections as well as for service access.

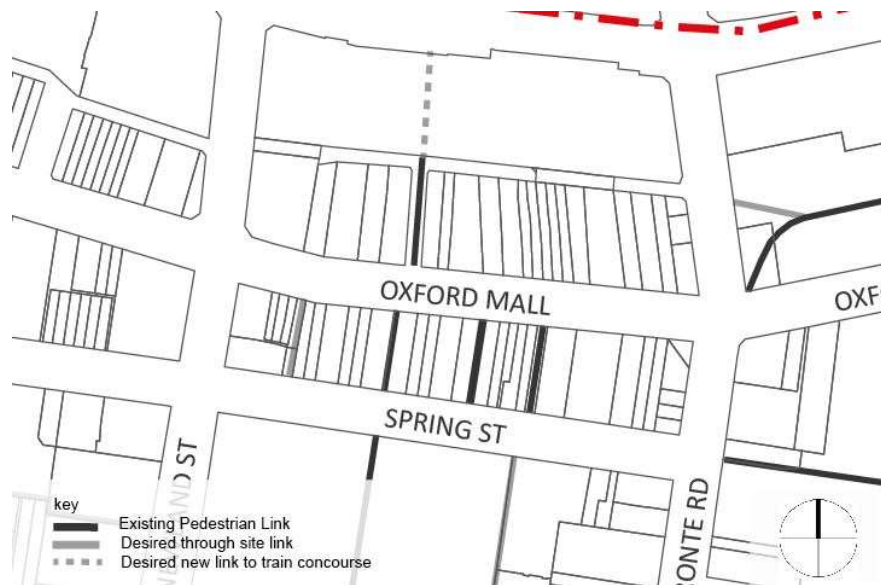
**Controls***General Controls*

- (a) Through site links, arcades, shared ways and laneways are to be provided as shown in Figure 383.
- (b) Retain all arcade connections and walkways.
- (c) Where possible, existing dead end lanes are to be extended through to the next street as redevelopment occurs and should provide clear sightlines from one end to the other.
- (d) New through site links should be connected with existing and proposed through site lanes, shared zones, arcades and pedestrian ways and opposite other through site links to enhance legibility to the whole laneway system.
- (e) Existing publicly and privately owned lanes are to be retained.

- (f) The design and finish of new through site links need to be provided in accordance with Council's *Public Domain Technical Manual for Bondi Junction Centre*.

#### *Pedestrian links*

- (g) Through site links for pedestrians are to be provided as shown in Figure 383 and have active ground floor frontages; be legible and direct thoroughways for pedestrians; provide public access at all business trading times or as otherwise stipulated by Council's conditions of approval; have a minimum width of 3m non-leasable space clear of all obstructions (including columns, stairs and escalators); where practicable, have access to natural light for at least 30% of their length; where air conditioned, have clear glazed entry doors comprising at least 50% of the entrance; have signage at street entries indicating public accessibility and the street to which the through site link connects; and maximise opportunities for integration of public art installations.
- (h) Internal arcades will not be approved in preference to activation of an existing or required lane. Where developments front a lane that is also a pedestrian route, provide an active frontage and design details that create visual interest such as landscaping, awnings, paved finishes and good lighting.
- (i) When a publicly accessible pedestrian connection is proposed to link directly to the railway line, Transport for NSW must approve connections to railway stations and approve designs. In addition, the developer will be required to enter into an agreement with Transport for NSW defining the controls to be implemented in managing access.
- (j) Due to its proximity to Bondi Junction Station and substantial foot traffic, Bronka Arcade is to have a minimum width of 6m, a minimum height of two storeys and have active frontages on both sides. (Note: A greater Floor Space Ratio and maximum building height is available for 153-165 Oxford Street in order to provide this through site link.)



**Figure 383** Through-site links

### 1.22.5 Active Street Frontages and Address

#### Objectives

- (a) To promote pedestrian activity and safety in the public domain.
- (b) To maximise active street fronts in Bondi Junction.
- (c) To define areas where active streets are required or are desirable.
- (d) To encourage an address to the street outside of areas where active street frontages are required.

#### Controls

##### *Active Street Frontages*

- (a) Provide active street frontages to all development identified in Figure 394 in accordance with *Part B16.2 Active Street Frontages*.

##### *Active frontage above Ground Floor*

- (b) Extend active frontages above ground floor level with uses and building design that provide transparency and visual contact with the street.
- (c) Integrate landscaping above ground floor levels to provide interest in design and amenity for uses of these spaces.

##### *Street Address*

- (d) Street address is defined as entries, lobbies, and habitable rooms with clear glazing to the street not more than 1.2m above street level and excluding car parking areas. Street address is required on Ground Level of all areas identified in Figure 394.
- (e) Provide multiple entrances for large developments including an entrance on each street frontage.

### 1.22.6 Awnings

#### Objectives

- (a) To increase pedestrian amenity by providing protection from wet weather and sunlight with awnings and colonnades.
- (b) To create a protected transition area between internal and external spaces for public and commercial buildings.
- (c) To improve pedestrian amenity by extending the footpath at ground floor level, and providing shelter and opportunities for outdoor dining.

#### Controls

- (a) Continuous street frontage awnings are to be provided for all new developments as indicated in Figure 394 in accordance with *B16.4 Awnings and Colonnades*.
- (b) Awning design must match building facades and be complementary to those of adjoining buildings.
- (c) Wrap awnings around corners for a minimum 6m from where a building is sited on a street corner.



Figure 39.4 Active street frontages required and front facade of buildings are to have awnings

### 1.22.7 Vehicle Footpath Crossings

#### Controls

##### *Location of Vehicle Access*

- No additional vehicle entry points will be permitted into the parking or service areas of development along those streets identified in Figure 40.35.
- In all other areas, one vehicle access point only (including the access for service vehicles and parking for non-residential uses within mixed use developments) will be generally permitted.

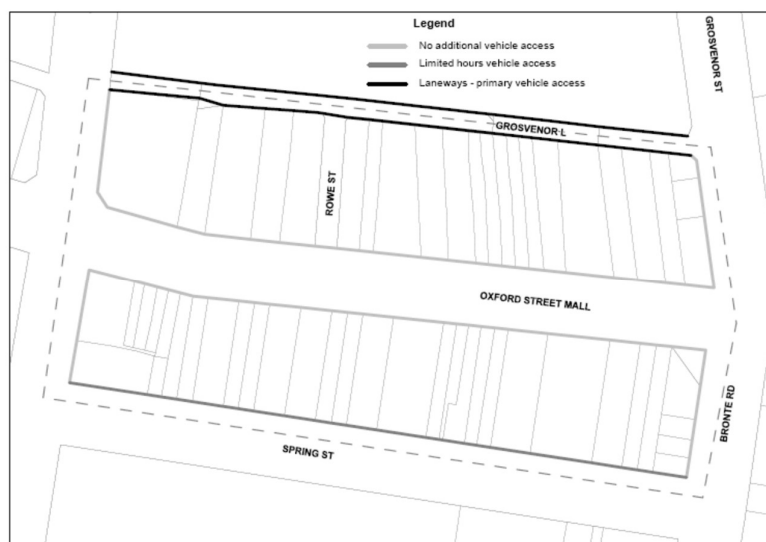


Figure 40.35 Vehicle access restrictions

### 1.22.8 Building Exteriors

#### Objectives

- (a) To contribute positively to the streetscape and public domain by means of high quality architecture and robust selection of materials and finishes.
- (b) To provide richness of detail and architectural interest especially at visually prominent parts of buildings such as lower levels and roof tops.
- (c) To present appropriate design responses to nearby development that complement the streetscape.
- (d) To clearly define the adjoining streets, street corners and public spaces and avoid ambiguous external spaces with poor pedestrian amenity and security.
- (e) To maintain a pedestrian scale in the articulation and detailing of the lower levels of the building.

#### Controls

- (a) Adjoining buildings (particularly heritage buildings) are to be considered in the design of new buildings in terms of: appropriate alignment and street frontage heights; setbacks above street frontage heights; appropriate materials and finishes selection; facade proportions including horizontal or vertical emphasis; and the provision of enclosed corners at street intersections.
- (b) Articulate façades so that these address the street and add visual interest.
- (c) External walls should be constructed of high quality and durable materials and finishes with 'self-cleaning' attributes, such as face brickwork, rendered brickwork, stone, concrete and glass.
- (d) Finishes with high maintenance costs, those susceptible to degradation or corrosion that result in unacceptable amenity impacts, such as reflective glass, are to be avoided.
- (e) To assist articulation and visual interest, avoid expanses of any single material.
- (f) Limit opaque or blank walls for ground floor uses to 30% of the street frontage.
- (g) Maximise glazing for retail uses, but break glazing into sections to avoid large expanses of glass.
- (h) Highly reflective finishes and curtain wall glazing are not permitted above ground floor level.
- (i) A materials sample board and schedule is required to be submitted with applications for development over \$1 million or for that part of any development built to the street edge.
- (j) The design of roof plant rooms and lift overruns is to be integrated into the overall architecture of the building.

### 1.22.9 Site Facilities and Services

#### Objectives

- (a) To ensure that site facilities are effectively integrated into the development and are unobtrusive.
- (b) To establish appropriate access and location requirements for servicing.
- (c) To ensure service requirements do not have adverse amenity impacts.



**Controls**

- (a) All site facilities are to be integrated into the design of the building.
- (b) For developments where a fire brigade vehicle is required to enter the site, vehicular access, egress and maneuvering must be provided to, from and on the site in accordance with the NSW Fire Brigades Code of Practice – Building Construction – NSWFB Vehicle Requirements. Generally, provision must be made for NSW Fire Brigade vehicles to enter and leave the site in a forward direction where: NSW Fire Brigade cannot park their vehicles within the road reserve due to the distance of hydrants from the building or restricted vehicular access to hydrants; or the site has an access driveway longer than 15m.

**1.22.10 Special Areas****Objectives**

- (a) The Rowe Street and Bronka Arcade sites, on the north and south sides of Oxford Street Mall, lie at the crossing of important pedestrian routes through Bondi Junction. These sites offer an opportunity to enliven Oxford Street Mall and the centre of the Bondi Junction Centre area more broadly, by activating the area through increased pedestrian traffic and enhanced permeability of the area.
- (b) On the Rowe Street site, a major new link is to be created between Oxford Street Mall and the railway interchange. This link will encourage greater pedestrian traffic on the Mall, improving the desirability of retail floor space in the Bondi Junction Centre.
- (c) On the Bronka Arcade site, existing connections are to be maintained and enhanced by a more generous, double height arcade connection to Spring Street.

**Controls***Built form*

- (a) On the Rowe Street site, new development must comply with the solar access plane illustrated in Figure 4136, which ensures that new development on this site does not overshadow the Oxford Street Mall in winter. On the Bronka Arcade site, new development is permitted to rise to 40m from the 9m setback to Oxford Street Mall.
- (b) *Public Domain Interface* In order to ensure a high quality interface with the retail frontage along its length, the covered arcade connecting Oxford Street Mall and the railway concourse/Tiffany Plaza development should have the same finished floor level as the mall, and should be level along its entire length. The connection down to the railway station and up to the Tiffany Plaza development should consist of escalators, stairs and a lift, contained within an arcade-style development with active frontages. Development on the Rowe Street site is to be serviced from Grosvenor Lane.
- (c) In order to encourage the activation of Spring Street, development on the Bronka Arcade site is to be serviced below ground, or on a limited hours basis from Spring Street.
- (d) Because of limited opportunities for vehicle access, new developments are encouraged to consolidate parking.

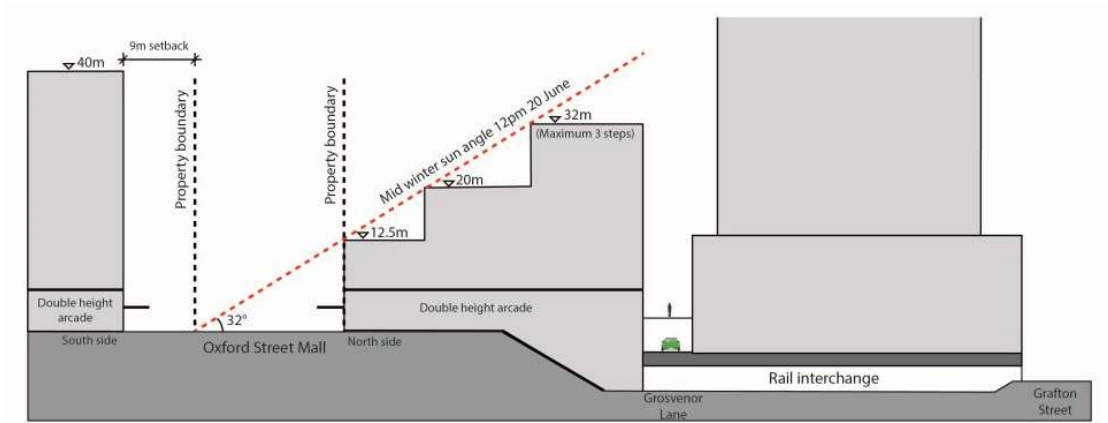


Figure 4136 Section showing intended Transport Interchange Upgrade

## E2 BOND BEACHFRONT AREA

Bondi Beach is an iconic location, including one of Australia's most famous beaches. Bondi Beach has local, state and national heritage significance and is a major tourist attractor as well as a popular spot for locals due to its eclectic character and services.

This Part plays an integral role in maintaining the area's unique qualities while providing urban design controls for residential and commercial development including controls relating to building height, parking, setbacks and building appearance.

This Part applies to land located within the Bondi Beachfront Area.

This Part applies to the land commonly known as the Bondi Beachfront Area shaded in Figure [4237](#).

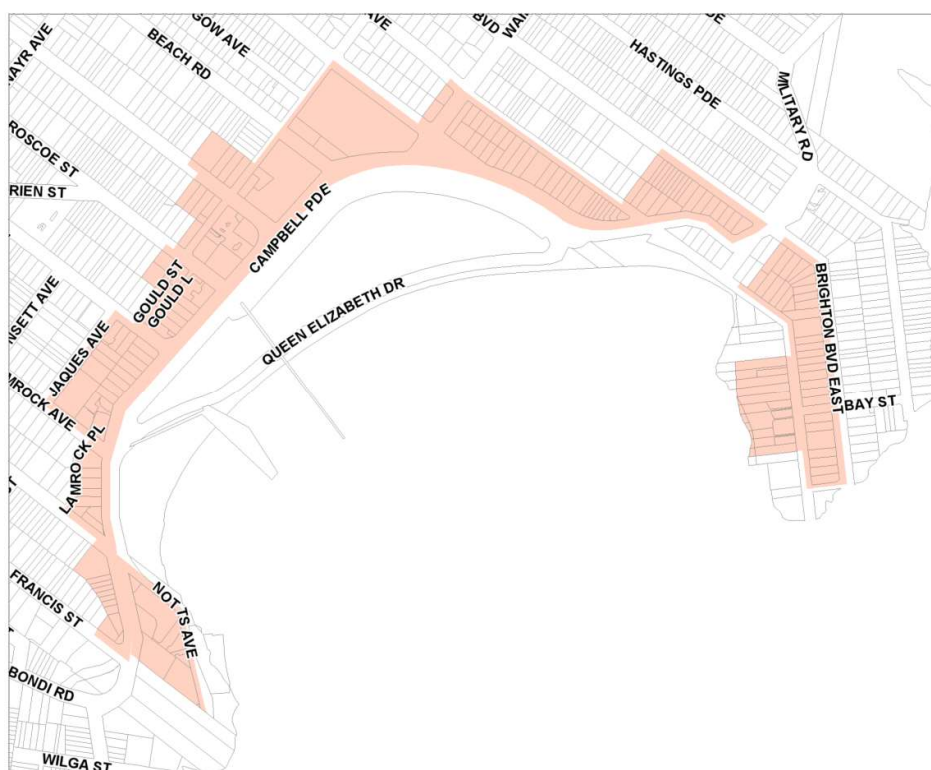


Figure [4237](#) Bondi Beachfront Area Centre

This Part contains general objectives and controls for development within the Bondi Beachfront Area as well as specific objectives and built form controls for five (5) Character Areas. A development is required to meet the general objectives and controls as well as the specific objectives and built form controls for the area in which the development is located.

## 2.1 GENERAL CONTROLS

This section outlines the general objectives and urban design controls that apply to the Bondi Beachfront Area. Where required, these controls must be read in conjunction with *Part E3 Local Village Centres*.

The controls should also be read in conjunction with the design guidelines included in *Annexure E2-1* which provide examples of building elements. These are derived from an analysis of the existing buildings. These guidelines are intended to guide owners to develop in a way that contributes to, and enhances, the individual character of the Bondi Beachfront Area.

### 2.1.1 Public Domain Interface

#### Objectives

- (a) To ensure priority is given to pedestrian movement.
- (b) To encourage retail trading and appropriate commercial uses at street level.
- (c) To encourage development with a strong street address and well-defined residential entries.
- (d) To ensure ground level building frontages are active, open, inviting and that shop fronts are maximised.
- (e) To provide continuous awnings for pedestrians in B4 – Mixed Use Zones.
- (f) To maintain the small shop character at ground floor in B4 – Mixed Use Zones.
- (g) To encourage publicly accessible through-site pedestrian access ways within B4 – Mixed Use Zones.
- (h) To provide safe pedestrian environments through reduced vehicular crossings on primary commercial streets.

#### Controls

- (a) Development must be sensitive to the streetscape character and views. A streetscape and context analysis is to be provided in accordance with Part B12 Design Excellence.
- (b) All development to which this Part applies is to provide active street frontages in accordance with *Part B16.2 Active Street Frontages*.
- (c) Buildings must have a clear street address with well-defined entries that are visible from the street.
- (d) Commercial and residential entries must be separated.
- (e) Access to residential dwellings above ground level should not occupy more than 20% of the principal street frontage.
- (f) New shop fronts must have proportions and characteristics that are consistent with other shop fronts in the Bondi Beachfront Area.
- (g) Shop fronts must consist primarily of clear glazing that is capable of opening to the public domain.
- (h) Opaque facades at ground level are not permitted in B4 – Mixed Use Zones.
- (i) Primary commercial street frontages must provide an entry to a retail premises every 5 - 6.5m
- (j) Vehicular entries are not permitted along Campbell Parade and primary commercial street frontages.

- (k) Pedestrian through-site access links are encouraged.

### 2.1.2 Building use

#### Objectives

- (a) To recognise the local role of the Bondi Beachfront Area.
- (b) To ensure that the Bondi Beachfront Area is not dominated by commercial and retail activity.
- (c) To ensure that the Bondi Beachfront Area maintains a high level of vibrancy.
- (d) To ensure that the Bondi Beachfront Area is afforded a high level of passive surveillance at all times.
- (e) To ensure continuous and active street frontages.

#### Controls

- (a) Consent must not be granted for development in relation to the use of a building erected or proposed to be erected on land in the Bondi Beachfront Area, if the Council is of the opinion that any part of a floor above the first floor will be used for the purpose of a Business Premises or Office Premises.
- (b) The ground floor and first floor of any development that is a building on land zoned B4 Mixed Use in the Bondi Beachfront Area as identified on the Area Map must have active street frontages and be used for retail premises, business premises, tourist and visitor accommodation or a combination of those uses.
- (c) The ground floor of any development that is a building on land zoned B4 Mixed Use in Hall Street or Curlewis Street must have active street frontages and be used for retail premises, business premises, or a combination of both.

### 2.1.3 Built Form

#### Objectives

- (a) To ensure new and refurbished buildings are sympathetic to the scale and height of existing buildings.
- (b) To reinforce the prevailing street pattern of rectilinear building forms as well as predominantly vertical proportions of bay openings and windows.
- (c) To maintain the existing building line abutting the street alignment along Campbell Parade.
- (d) To ensure built form does not negatively impact on the access to sunlight in public open spaces.
- (e) To ensure buildings provide high quality internal environments for occupants and users of the building, both residential and non-residential uses.
- (f) To maintain the scale and alignment of the existing predominant street wall.

#### Controls

- (a) The built form of new and refurbished buildings must complement the height and scale of the prevalent built form within the Bondi Beachfront Area.
- (b) Where a building façade adjoins a heritage item or a contributory building, it must have a façade that complements the form and proportion of the building.

- (c) Buildings along Campbell Parade must be built to the street alignment and predominant surrounding street wall height.
- (d) Attic levels must be wholly contained within a hipped or gabled roof form, and should be setback a minimum of three metres from the principal façade and not encroach into the setback line.
- (e) External sun shading must be consistent with the style and articulation of the building. Sun shading must not project beyond the principal façade.
- (f) For non-residential uses, habitable floorspace should be more than 10m from a window.
- (g) Buildings in the B4 – Mixed Use zone must provide a minimum of 3.3m clear ceiling heights on Ground Level and Level 1.
- (h) Corner sites require architectural treatment which emphasizes the prominent role filled by these sites. Measures include the deletion of upper floor setbacks with construction to external site boundaries, design measures to emphasize the corner and improvement to the public domain.
- (i) Openings to new balconies in existing facades should not exceed the width of existing openings and make use of existing openings where possible.
- (j) Voids or gaps in the street wall should be avoided.
- (k) For sites adjoining residential zoned land, the building is to be setback a minimum of 1.5m from the common boundary.
- (l) Commercial and retail buildings are to comply with the following:
  - (i) Minimum floor to ceiling height of 3.6m above ground floor,
  - (ii) Minimum 4m floor to ceiling height at ground floor.



### 2.1.4 Roofs

#### Objectives

- (a) To maintain the established roof-scape along Campbell Parade.
- (b) To ensure rooftop elements are cohesive with the existing streetscape and their roof mounted services are concealed from and do not dominate roof-scapes viewed from Campbell Parade, Bondi Beach or the public domain.
- (c) Encourage solar collectors and photovoltaic cells to be integrated into the overall design of roof terraces.
- (d) To ensure that balconies and balcony or roof top additions do not substantially alter heritage items or contributory buildings.

#### Controls

- (a) The existing pattern of roof forms and roof elements along Campbell Parade must be retained.
- (b) Rooftop elements and buildings services located on the roof of a building must not be visible at eye level, 1.5m above the existing finished ground level, when viewed from the property boundary opposite the site.

### 2.1.5 Views

#### Objectives

- (a) To protect and enhance views from the public domain.
- (b) To minimise view loss from existing developments by proposed development.
- (c) To promote the concept of view sharing as a means of ensuring equitable access to views.

#### Controls

- (a) Proposed development must respect existing view corridors from the public domain.
- (b) Proposed development should avoid impacting on existing views where possible.
- (c) Trees are not permitted from being planted where they would take away an existing view from the habitable room or balcony of an existing building when mature.

### 2.1.6 Heritage Conservation

*Part B89 Heritage applies for the Bondi Beachfront Area.*

#### Objectives

- (a) To protect and enhance heritage items, contributory buildings and the established character of the heritage urban conservation area.
- (b) To enable ongoing adaptive reuse of heritage items and contributory buildings where existing usage is no longer viable.

- (c) To ensure retention and restoration of detailing to heritage items and contributory buildings including street level shopfronts and entry lobbies to residential flat buildings.
- (d) To ensure heritage items and contributory buildings are retained and remain legible as individual buildings in new developments.

#### Controls

- (a) Heritage items and contributory buildings are nominated on the Heritage Items and Contributory Buildings Maps provided for each Character Area in *Part E2.2 Character Areas*.
- (b) Heritage items and contributory buildings are to be retained and to remain legible as individual buildings in any related development.
- (c) Heritage items and contributory buildings may be adaptively reused where existing usage is no longer viable.
- (d) Adaptive reuse of heritage items and contributory buildings is to maintain the form, detail and finishes of the existing buildings as the dominant aspect of the site with new works having limited impact upon the significance and contribution of the building to the conservation area.
- (e) Any works adjacent to or in the context of heritage items and contributory buildings must clearly demonstrate cohesion with the existing historic character of the streetscape and the form, alignment, detailing, articulation and materials of heritage items and contributory buildings defining the conservation area.
- (f) Where upper storey additions are proposed to heritage items or contributory buildings that have pitched roofs, attic additions are to be utilised in lieu of additional expressed floors.

#### 2.1.7 Infill Buildings

*Part B89 Heritage* also applies for the Bondi Beachfront Area

#### Objectives

- (a) To encourage infill buildings sympathetic in style to heritage items and contributory buildings in the Bondi Beachfront Area.
- (b) To discourage infill buildings from imitating characteristics of heritage items and contributory buildings.

#### Controls

- (a) Infill buildings must not imitate decorative details or features of heritage item and contributory buildings.
- (b) Fenestrations must have similar proportions to heritage items and contributory buildings within the Bondi Beachfront Area.
- (c) Where a new building is located adjacent to heritage items or contributory buildings, its design must be sympathetic in scale, alignment, detailing and materials to these existing buildings.
- (d) Infill buildings must build to the prevailing street wall height then setback a minimum of 3m to any upper floors.

## 2.2 CHARACTER AREAS

The Bondi Beachfront Area contains a number of areas that contain similar characteristics and development potential and are known as Character Areas as shown in Figure 4338 and includes:

- A - Notts Avenue;
- B - Campbell Parade South;
- C - Campbell Parade Centre;
- D - Campbell Parade North; and
- E - Ramsgate Avenue East.

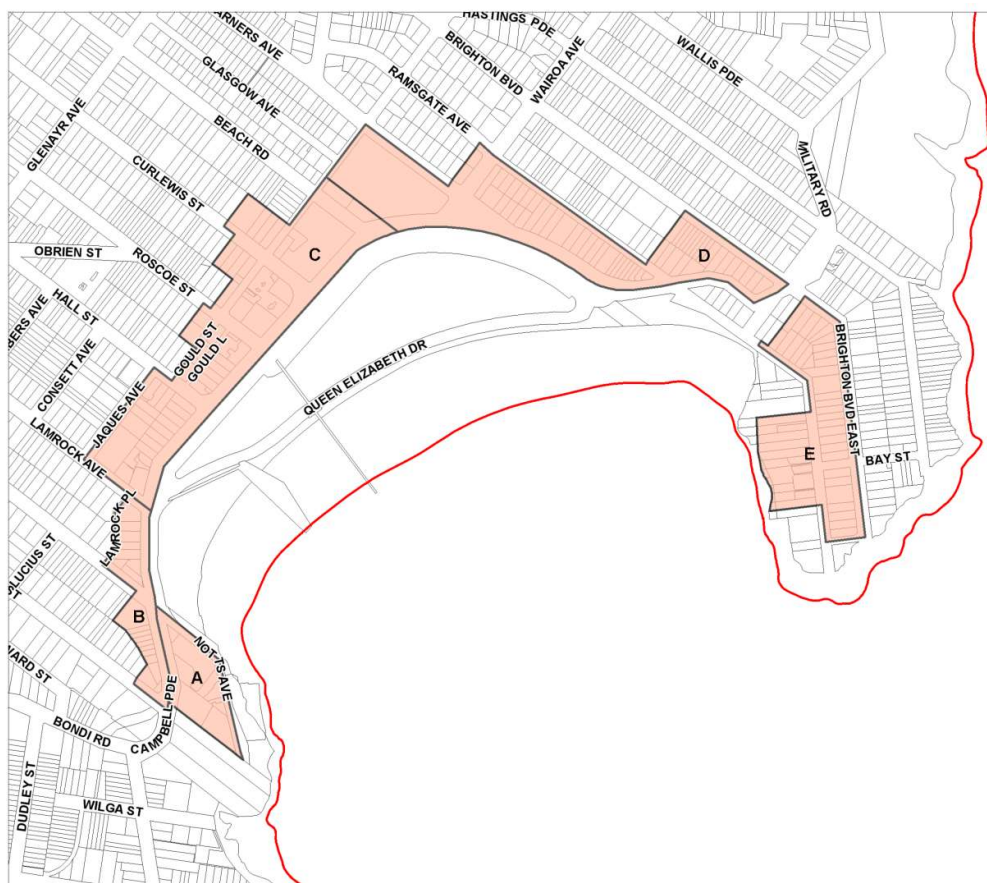


Figure 4338 Character Areas

### 2.2.1 Notts Avenue

#### Existing Character and Built Form

Notts Avenue is a residential area comprising a variety of housing forms including street defining residential flat buildings along Notts Avenue and a tower on top of the headland along Campbell Parade (refer to Figure 39). Buildings are generally oriented to the north to take advantage of the aspect and elevated views over Bondi Beach. It has an irregular subdivision pattern.

Existing buildings in this area are predominantly rendered masonry with flat and pitched roofs. Buildings have a variety of expressions from the strongly horizontal emphasis of the building at the corner of Notts Avenue and Campbell Parade to vertical flat buildings.

Notts Avenue is characterised by blank street walls and a raised pedestrian footway at ground level with some garage openings and buildings located on top of a sloping topography. The Campbell Parade frontage is not street defining and contains residential uses at ground level.

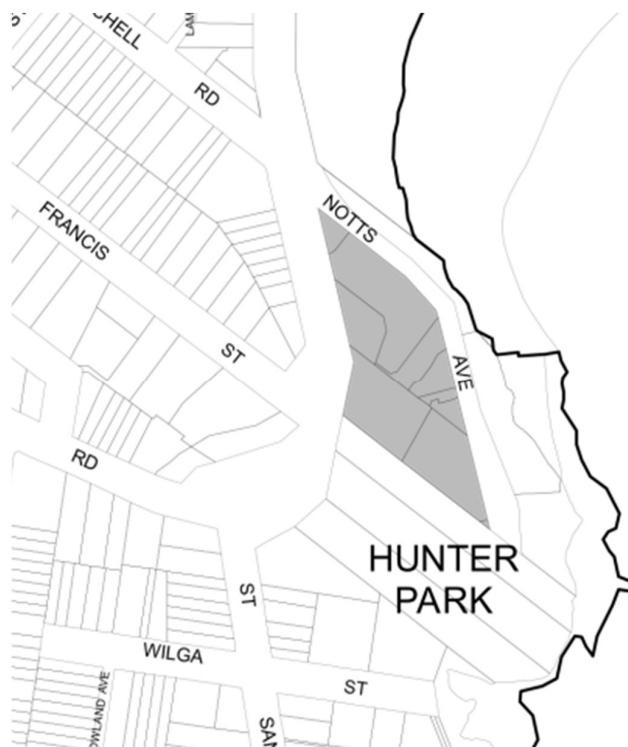


Figure 4439 Notts Avenue Character Area

**Desired Future Character Objectives**

- (a) To maintain a residential character and support a diversity of residential accommodation in the area.
- (b) To ensure that vehicular entries do not dominate Notts Avenue.
- (c) To retain established building levels along Notts Avenue.
- (d) To encourage buildings along Campbell Parade and Notts Avenue to be built to the street edge with no setbacks.

**Controls**

- (a) Land use
  - (i) Developments are to retain the predominantly residential character of the area.
  - (ii) Developments fronting Campbell Parade are encouraged to have active ground floor frontages with retail.
- (b) Height and Bulk
  - (i) A maximum of 3 storeys is permitted.
  - (ii) A maximum external wall height of 10m is permitted.
- (c) Setbacks
  - (i) Buildings fronting Campbell Parade are to be built to the street edge with no setbacks.
  - (ii) Buildings are to provide sufficient rear setbacks to provide courtyards.
  - (iii) Buildings fronting Campbell Parade must have zero side setbacks for min. 10m from the Campbell Parade street wall for the height of the street wall.
- (d) Façade Materials and Finishes
  - (i) Blank, flat and unarticulated facades are not permitted.
  - (ii) Buildings must not use materials that are highly reflective.
  - (iii) Windows must be composed as part of the overall form of the building.
  - (iv) Dark or tinted glazing is not permitted.
- (e) Balconies and Balustrades
  - (i) Balconies along Campbell Parade must be recessed into the building envelope and should not project forward of the principal façade.
  - (ii) Balconies along Campbell Parade must be screened.
  - (iii) Balconies adjacent to a public open space or on side boundaries must be screened.
  - (v) Balconies must be designed as part of the overall form of the building.
  - (vi) All balustrades, except those along Campbell Parade, must be predominantly constructed of clear, semi-frameless glazing.

- (f) External Sun Shading
  - (i) External sun shading must be constructed of materials that are suitable to the environmental conditions of the site.
  - (ii) External sun shading must be consistent with the style and articulation of the building.
- (g) Roofs and Parapets
  - (i) Roofs must be flat and edged by parapets along Campbell Parade and at the corners with Notts Avenue and Hunter Park for 10 metres back from the corner.
- (h) Façade Colours
  - (i) Light to mid colours must be used.
  - (ii) Dark colours are not permitted.
- (i) Awnings
  - (i) Awnings are required along Campbell Parade.
- (j) Parking
  - (i) Car parking should be located below ground level and should not be visible from the street.
  - (ii) Car parking access via Campbell Parade is not permitted.
  - (iii) Car parking at or above ground level is discouraged. If there is no alternative - it should be screened behind habitable uses to a minimum depth of 8 metres. Car parking must not be visible from the street or from a public place



### 2.2.2 Campbell Parade South

#### Existing Character and Built Form

Campbell Parade is the principal street that follows the curve of Bondi Beach and is an integral element of the tourist image, providing retail, food and other services for the transient day/night time population, short-term residents and local community (refer to Figure 459). A regular pattern of secondary streets run perpendicular to Bondi Beach, creating visually prominent corners at Francis Street, Sir Thomas Mitchell Road and Lamrock Avenue. The land slopes steeply from Sir Thomas Mitchell Road to the top of the southern headland at Hunter Park.

Existing buildings have narrow frontages built to the street alignment, with notable facades that contribute to its Interwar heritage. Buildings are predominantly rendered masonry with parapets and a vertical expression through the use of bay or vertically proportioned windows, pilasters and a few balconies, typically enclosed. Existing buildings generally have a north-eastern orientation that takes advantage of the aspect and views over Bondi Beach.

Many sites contain heritage items and a large proportion of the area is located within the heritage urban conservation area. Many sites contain contributory buildings worthy of retention as they contribute to the overall character of the Area. These buildings are generally intact and consistent with other 1920s/30s precincts in Sydney.



Figure 459 Campbell Parade South Character Area

**Desired Future Character Objectives**

- (a) To support and maintain the iconic role and unique character of the Campbell Parade retail strip as a separate area within the wider Bondi Beach Town Centre in providing local shops, services and residential accommodation for day visitors and the local community.
- (b) To maintain the mixed-use character of the centre by locating small shops and services at ground level and level one with a diversity of residential accommodation above.
- (c) To encourage outdoor seating on top of awnings along Campbell Parade.
- (d) To ensure new development and major renovations are consistent with the existing character of the area.
- (e) To encourage development that addresses the street and is built to the street alignment along Lamrock Place.

**Controls**

- (a) Land use
  - (i) Developments are to retain the mixed use character of the area by locating commercial at ground and level 1 and residential above.
- (b) Height and Bulk
  - (i) A maximum of 4 storeys is permitted.
  - (ii) A maximum external wall height of 12.5m is permitted.
- (c) Setbacks
  - (i) Buildings are to be built to the street edge with no setbacks.
  - (ii) Buildings are to be built to the side boundaries for minimum 10m from the front street wall.
  - (iii) Balconies and terraces may extend over the ground floor awning where commercial is proposed.
  - (iv) Where a building is to be extended by the construction of additional floors, the new section is to be setback from the existing façade line by a minimum distance of 3m.
  - (v) Attic levels or part additional floors must be setback minimum 3 metres from the street wall.
- (d) Heritage and contributory buildings
  - (i) Maintain the existing character of the area including narrow frontages and vertical expression.
  - (ii) Where a building is to be constructed in conjunction with a retained façade, the new construction is to be setback and integrated with the preserved section of the building.
  - (iii) Corner sites require architectural treatment which emphasises the prominent role filled by these sites in the urban context.
- (e) Façade Materials and Finishes
  - (i) New facades must be predominately rendered masonry with parapets and have a vertical expression.
  - (ii) Blank, flat and unarticulated facades are not permitted.

- (iii) Access to residential dwellings above ground level should not occupy more than 20% of the principal street frontage of any development.
  - (iv) Developments on corner sites are to be designed to accentuate the corner and provide a transition between one streetscape and the next.
  - (v) Windows above ground level must have vertical proportions.
  - (vi) Windows should be integral with the façade and not applied decoration.
  - (vii) Dark or tinted glazing is not permitted.
- (f) Balconies and Balustrades
  - (i) Balconies along Campbell Parade must be recessed into the building envelope and should not project forward of a principal façade.
  - (ii) Balustrades along Campbell Parade, must be predominantly solid with no or minimal glazing.
  - (iii) Balconies must be composed as part of the overall form of the building.
  - (iv) All balustrades, except those along Campbell Parade, must be predominantly constructed of clear, semi-frameless glazing.
- (g) External Sun Shading
  - (i) External sun shading must be constructed of materials that are suitable to the environmental conditions of the site.
  - (ii) External sun shading must be consistent with the style and articulation of the building.
  - (iii) Sun shading must not project beyond the principal façade.
- (h) Roofs and Parapets
  - (ii) Parapets must be predominantly rendered masonry.
  - (iii) Roofs must be flat with parapets.
  - (iv) Roofs must not be visible from Campbell Parade, unless there is a contextual reason for providing a pitched roof to relate to an adjacent heritage item or contributory building.
  - (v) The roofline of buildings, predominately comprising lift motor rooms and plant rooms shall be designed as an integral part of the buildings architectural form.
- (i) Façade Colours
  - (i) Colours should be consistent with, retained or reinstated on heritage items and contributory buildings (refer to *Annexure E2-1*).
  - (ii) Light to mid colours must be used on all other buildings.
  - (iii) Dark colours are not permitted.
- (j) Parking
  - (i) Vehicle entries are not permitted along Campbell Parade.
  - (ii) Where parking is permitted, it should be located below ground level and should not be visible from the street.

### 2.2.3 Campbell Parade Centre

#### Existing Character and Built Form

Campbell Parade is the principal street that runs parallel to Bondi Beach. Gould Street and Jacques Avenue are secondary streets that run parallel to Campbell Parade (refer to Figure 461). A regular pattern of secondary streets run perpendicular to Campbell Parade, creating visually prominent corners at Lamrock Avenue, Hall Street, Curlewis Street and Beach Road.

Campbell Parade is an integral element of the tourist image, providing retail, food and other services for the transient day/night time population and local community. Gould Street is an increasingly vibrant secondary street, providing specialist retail for visitors and the surrounding neighbourhood.

Buildings between Roscoe Street and Lamrock Avenue have narrow frontages and are built to the street alignment, with notable facades that contribute to its Interwar heritage. These buildings are predominantly rendered masonry with parapets with a vertical expression through the use of bay or vertically proportioned windows, pilasters and few balconies, typically enclosed. Existing buildings generally have a south-eastern orientation that takes advantage of the views over Bondi Beach, generally without balconies.

Many sites contain heritage items or contributory buildings and a large proportion of the area is located within the heritage urban conservation area. These buildings are generally intact and consistent with other 1920s/30s precincts in Sydney.



Figure 461 Campbell Parade Centre Character Area

**Desired Future Character Objectives**

- (a) To support and maintain the iconic role and unique character of the Campbell Parade retail strip as a separate area within the wider Bondi Beach Town Centre in providing local shops, services and residential accommodation for day visitors and the local community.
- (b) To increase access links between Campbell Parade and Gould Street to encourage pedestrian movement that supports local shops and increase the retail frontage.
- (c) To maintain the mixed-use character in the centre by locating small shops and services at ground level and level one with a diversity of residential accommodation above.
- (d) To ensure new development and major renovations are consistent with the existing character of the area.
- (e) To minimise heritage impacts on identified heritage items and conservation areas within this and adjoining areas.

**Controls**

- (a) Land use
  - (i) Developments are to retain the mixed use character of the area by locating commercial at ground and 1<sup>st</sup> floor level and residential above.
  - (ii) New developments should provide pedestrian through site access links between Campbell Parade and Gould Street.
- (b) Height and Bulk
  - (i) A maximum of 4 storeys is permitted except for buildings fronting Curlewis Street, Beach Road or the western side of Gould Street where a maximum of 3 storeys is permitted.
  - (ii) A maximum external wall height of 12.5m is permitted except for buildings fronting Curlewis Street, Beach Road or the western side of Gould Street where a maximum of 10m is permitted.
  - (iii) An attic level or part additional floor may be permitted.
- (c) Setbacks
  - (i) Buildings within the B4 – Mixed Use zone are to be built to the street edge with no setbacks.
  - (ii) Buildings are to be built to the side boundaries for a minimum of 10m from the front street wall
  - (iii) Where a building is to be extended by the construction of additional floors, the new section is to be setback from the existing façade line by a minimum distance of 3m.
  - (iv) Attic levels or part additional floors must be setback minimum 3 metres from the street wall.
- (d) Heritage and contributory buildings
  - (i) Maintain the existing character of the area including narrow frontages and vertical front facade expression.
  - (ii) Where a building is to be constructed in conjunction with a retained façade, the new construction is to be similarly setback and integrated with the preserved section of the building.

- (iii) Corner sites require architectural treatment which emphasises the prominent role filled by these sites in the urban context.
- (e) Façade Materials and Finishes
  - (i) New facades must be predominately rendered masonry with solid parapets and have a vertical expression.
  - (ii) Blank, flat and unarticulated facades are not permitted.
  - (iii) Access to residential dwellings above ground level should not occupy more than 20% of the principal street frontage of any development.
  - (iv) Developments on corner sites are to be designed to accentuate the corner and provide a transition between one streetscape and the next.
  - (v) Fenestrations above ground level must have a vertical proportion, unless the existing character is otherwise.
  - (vi) Dark or tinted glazing is not permitted.
- (f) Balconies and Balustrades
  - (i) Balconies along Campbell Parade must be recessed into the building envelope and should not project forward of a principal façade.
  - (ii) Balustrades along Campbell Parade, must be predominantly solid with no or minimal glazing.
  - (iii) Balconies adjacent to a public open space or on side boundaries must be screened.
  - (iv) Balconies must be composed as part of the overall form of the building.
  - (v) All balustrades, except those along Campbell Parade, must be predominantly constructed of clear, semi-frameless glazing.
- (g) External Sun Shading
  - (i) External sun shading must be constructed of materials to suit the environmental conditions of the site.
  - (ii) External sun shading must be consistent with the style and articulation of the building. Sun shading must not project beyond the principal façade.
- (h) Roofs and Parapets
  - (i) Parapets must be predominantly rendered masonry.
  - (ii) Roofs must be flat with parapets.
  - (iii) Roofs must not be visible from Campbell Parade, unless there is a contextual reason for providing a pitched roof to relate to an adjacent heritage item or contributory building.
  - (iv) The roofline of buildings, predominately comprising lift motor rooms and plant rooms shall be designed as an integral part of the buildings architectural form.
- (i) Façade Colours
  - (i) Colours should be consistent with, retained or reinstated on heritage items and contributory buildings (refer to *Annexure E2-1*).
  - (ii) Light to mid colours must be used on all other buildings.
  - (iii) Dark colours are not permitted.
- (j) Awnings
  - (i) New awnings must step to reflect the topography.

- (k) Parking
  - (i) Vehicle entries are not permitted along Campbell Parade.
  - (ii) Parking should be located below ground level and should not be visible from the street.



### 2.2.4 Campbell Parade North

#### Existing Character and Built Form

Campbell Parade is the principal street that follows the gentle curve of Bondi Beach. A regular pattern of secondary streets runs perpendicular with the Campbell Parade retail strip. The land is steeply sloping towards Dover Heights and the secondary streets generally run along the contours (refer to Figure 472).

This area has a variety of building types including dwelling-houses, townhouses and residential flat buildings. Shop-top housing is generally located towards the corners.

Existing buildings along Campbell Parade have narrow frontages and are built to the street alignment, with notable facades that contribute to its Interwar heritage. Many sites contain contributory buildings which contribute to the overall character of the Area. These buildings are generally intact and consistent with other 1920s/30s precincts in Sydney.

Existing buildings are predominantly rendered masonry with parapets with a vertical expression through the use of bay or vertically proportioned windows, pilasters and few balconies, typically enclosed.

Buildings are generally oriented to the south to take advantage of the view over Bondi Beach, with some balconies. The orientation and narrow frontages limit solar access and cross ventilation.



Figure 472 Campbell Parade North

**Desired Future Character Objectives**

- (a) To support the unique mixed use character of this section of Campbell Parade.
- (b) To discourage residential accommodation at street level along Campbell Parade.
- (c) To ensure new development and major renovations are consistent with the existing character of the area.
- (d) To ensure development is built to the street with no setbacks along Campbell Parade.

**Built Form Controls**

- (a) Land use
  - (i) Developments are to retain the predominantly residential character of the area with retail at street level encourages with properties fronting Campbell Parade.
- (b) Height and Bulk
  - (i) A maximum of 4 storeys is permitted.
  - (ii) A maximum external wall height of 12.5m is permitted.
  - (iii) An attic level or part additional floor may be permitted.
  - (iv) New buildings must address the character of adjoining buildings and generally reproduce the side setbacks, bulk and scale of adjoining built form.
- (c) Setbacks
  - (i) Buildings with frontages to Campbell Parade are to be built to the street edge with no setbacks.
  - (ii) Buildings with frontages to Ramsgate Avenue and Brighton Boulevard are to have a front setback of 3m.
  - (iii) Buildings are to provide front and rear setback back for floors above street level to provide balconies.
  - (iv) Balconies and terraces may extend over the ground floor awning where commercial is proposed.
  - (v) Where a building is to be extended by the construction of additional floors, the new section is to be setback from the existing façade line by a minimum distance of 3m.
- (d) Façade Materials and Finishes
  - (i) New facades must be predominately rendered masonry with parapets and have a vertical expression.
  - (ii) Blank, flat and unarticulated facades are not permitted.
  - (iii)
- (e) Heritage and contributory buildings
 

are to conform to the following controls:

  - (i) Maintain the existing character of the area including narrow frontages and vertical front facade expression.
  - (ii) Where a building is to be constructed in conjunction with a retained façade, the new construction is to be similarly setback and integrated with the preserved section of the building.
  - (iii) Existing face brick building exteriors should be retained and not painted or rendered.

- (f) Façade Materials and Finishes
  - (i) New facades must be predominately rendered masonry with parapets and have a vertical expression.
  - (ii) Blank, flat and unarticulated facades are not permitted.
  - (iii) Buildings within the visual catchment of Bondi Beach must not use materials that are highly reflective.
  - (iv) Windows above ground level must have a vertical proportion.
  - (v) Dark or tinted glazing is not permitted.
- (g) Balconies and Balustrades
  - (i) Balconies along Campbell Parade must be recessed into the building envelope and should not project in front of a principal façade.
  - (ii) Balustrades along Campbell Parade, must be predominantly solid with no or minimal glazing.
  - (iii) Balconies adjacent to a public open space or on side boundaries must be screened.
  - (iv) Balconies must be composed as part of the overall form of the building.
- (h) External Sun Shading
  - (i) External sun shading must be suitable to the environmental conditions of the site.
  - (ii) External sun shading must be consistent with the style and articulation of the building. Sun shading must not project beyond the principal façade.
- (i) Roofs and Parapets
  - (i) Parapets must be predominantly rendered masonry.
  - (ii) Roofs must be flat with parapets.
  - (iii) Roofs must not be visible from Campbell Parade, unless providing a pitched roof relates to an adjacent heritage item or contributory building.
  - (iv) The roofline of buildings, predominately comprising lift motor rooms and plant rooms shall be designed as an integral part of the buildings architectural form.
- (j) Façade Colours
  - (i) Colours must be consistent with, retained or reinstated on heritage items and contributory buildings (refer to *Annexure E2-1*).
  - (ii) Light to mid colours must be used on all other buildings.
  - (iii) Dark colours are not permitted.
- (k) Awnings
  - (i) Awnings must be provided where there are retail uses at ground floor.
- (l) Parking
  - (i) Vehicle entries are not permitted along Campbell Parade.
  - (ii) Parking should be located below ground level and should not be visible from the street.
  - (iii) Car parking should not take the place of shop fronts at street level.

### 2.2.5 Ramsgate Avenue East

#### Existing Character and Built Form

The area generally has a regular subdivision pattern with narrow frontages to the street. It is a residential area comprising a variety of housing including dwelling-houses, two to three storey townhouses and residential flat developments. Buildings are generally oriented towards the west to take advantage of the elevated views over Bondi Beach. There is some shop-top housing at the western Ramsgate Avenue East, opposite Biddigal Reserve.

Existing buildings in this area are predominantly masonry, rendered and face brick, with pitched roofs but there are some flat and curved roofs. They have a variety of expressions with large fenestrations to the west and balconies are common. There are no heritage items in the area and it is located outside the heritage urban conservation area. Many sites contain buildings that are worthy of retention as they contribute to the overall character of the Area.

Brighton Avenue East has wide landscape strip with street trees with buildings setback from the street (refer to Figure 483).



Figure 483 Ramsgate Avenue East Character Area

**Desired Future Character Objectives**

- (a) To maintain the residential character of the area and support a diversity of residential accommodation, with some shops at ground level opposite Biddigal Reserve.
- (b) To encourage development to address the street on the low-side of Ramsgate Avenue East.
- (c) To encourage built form with a vertical expression, constructed primarily of masonry with a consistent street wall height and attic levels setback from the street with balconies.
- (d) To discourage uncovered car parking and carports within the front setback.
- (e) To ensure that balconies and bay windows on side boundaries maintain visual and acoustic privacy between buildings.
- (f) To encourage balconies and operable screens that are integrated into the overall design of the building and that are constructed of materials appropriate to the exposed site conditions.
- (g) To maintain existing building setbacks.
- (h) To maintain and enhance existing view corridors.

**Built Form Controls**

- (a) Land use
  - (i) Developments are to retain the residential character of the area.
- (b) Height and Bulk
  - (i) A maximum of 3 storeys is permitted for buildings fronting Ramsgate Avenue and Brighton Boulevard.
  - (ii) Additional storeys are permitted where properties have dual frontage to Ramsgate Avenue East and the Coastline or where the topography permits.
  - (iii) An attic level or part additional floor may be permitted.
- (c) Setbacks
  - (i) Buildings are to have a minimum front setback equal to the average setback of the adjoining two houses on each side and 3m for properties fronting Ramsgate Avenue East.
  - (ii) Buildings are to provide rear setbacks for floors above street level to provide balconies. Where a building is to be extended by the construction of additional floors, the new section is to be setback from the existing façade line by a minimum distance of 3m.
- (d) Façade Materials and Finishes
  - (i) New facades must be predominately rendered masonry with a vertical expression.
  - (ii) Blank, flat and unarticulated facades are not permitted.
  - (iii) Buildings within the visual catchment of Bondi Beach must not use materials that are highly reflective.
- (e) Heritage items and contributory buildings
  - (i) Maintain the existing character of the area including narrow frontages and vertical expression.

- (ii) Where a building is to be constructed in conjunction with a retained façade, the new construction is to be similarly setback and integrated with the preserved section of the building.
  - (iii) Existing face brick building exteriors should be retained and not painted or rendered.
- (f) Fenestrations
  - (i) Fenestrations must have a vertical proportion.
  - (ii) Dark or tinted glazing is not permitted.
  - (iii) Fenestrations along a side boundary must ensure visual and acoustic privacy is maintained between buildings.
- (g) Balconies and Balustrades
  - (i) Balustrades fronting the coastline must be predominantly solid with no or minimal glazing.
  - (ii) Balconies must be composed as part of the overall form of the building.
  - (iii) Multiple balconies must be arranged with a vertical expression.
  - (iv) Balconies along the coastline must be recessed into the building envelope and should not project in front of the principal façade.
  - (v) Balconies adjacent to a public open space or on side boundaries must be screened.
- (h) External Sun Shading
  - (i) External sun shading must be consistent with the style and articulation of the building.
  - (ii) Sun shading must not project beyond the principal façade.
- (i) Roofs and Parapets
  - (i) The roofline of buildings, predominately comprising lift motor rooms and plant rooms shall be designed as an integral part of the buildings architectural form.
- (j) Façade Colours
  - (i) Colours should be consistent with, retained or reinstated on heritage items and contributory buildings (refer to *Annexure E2-1*).
  - (ii) Light to mid colours should be used on all other buildings.
  - (iii) Dark colours should be avoided.
- (k) Awnings
  - (i) Awnings are not permitted.
- (l) Parking
  - (i) Car parking at ground level is discouraged. If there is no alternative, it should be screened behind habitable uses to a minimum depth of 8 metres.
  - (ii) Car parking must not be visible from the street or from a public place.

## E3 LOCAL VILLAGE CENTRES

Throughout Waverley there are a number of local village centres. These are smaller centres serving the local community, separate to the regional role of Bondi Junction and Campbell Parade at Bondi Beach.

The centres enjoy unique position and character. Some of these centres are small, but all provide valuable services and facilities to local residents and users.

The purpose of this part is to strike a balance between upgrading and improving the public and private domain in village centres, while maintaining their character and affordability.

The zones reflect a hierarchy, where B1 Neighbourhood Centre is a cluster of shops, B2 Local Centre is a larger centre or high-street strip of shops, B3 Commercial Core is a major centre with office buildings and major retail, while B4 Mixed Use supports a mix of commercial and residential particularly in strategic centres around a B3 Commercial Core zone.

CENTRE NAME	CENTRE TYPE
<b>Bronte Road Corridor</b>	
Bronte Road, Bondi Junction	Strategic Centre
Charing Cross	Local Centre
Macpherson Street	Neighborhood Centre
Bronte Beach	Neighborhood Centre
Belgrave Street	Neighborhood Centre
<b>Old South Head Road Corridor</b>	
Flood Street	Neighborhood Centre
Curlewis Street	Local Centre
OSH Road, at Murriverie Road	Neighborhood Centre
Rose Bay South	Local Centre
Blake Street	Neighborhood Centre
Rose Bay North	Local Centre
Murriverie Road	Neighborhood Centre
Vaucluse	Neighborhood Centre
<b>Bondi Road Corridor</b>	
Bondi Road	Local Centre
Fletcher Street	Neighborhood Centre



Bondi Beach	Local Centre
Seven Ways	Neighborhood Centre
North Bondi	Neighborhood Centre
Wairoa Avenue	Neighborhood Centre

**Table 1** Local Village Centres and centre hierarchy

**Note:** Planning controls and objectives for the Bronte RSL site at 113 Macpherson Street, Bronte are in *Part E4 113 Macpherson Street, Bronte* of this DCP.



**Figure 49** Village Centres (Our Liveable Places Centres Strategy)

### 3.1 VILLAGE CENTRE SPECIFIC CONTROLS

This section provides an outline of the 19 [excluding Bondi Beachfront Area and Bondi Junction] identified village centres and detail the desired future character of each centre. The desired future character is to be taken into consideration when designing the built form of the proposed development.

#### 3.1.1 Bronte Road, Bondi Junction

Part E1 Bondi Junction also applies to the Bronte Road village centre.

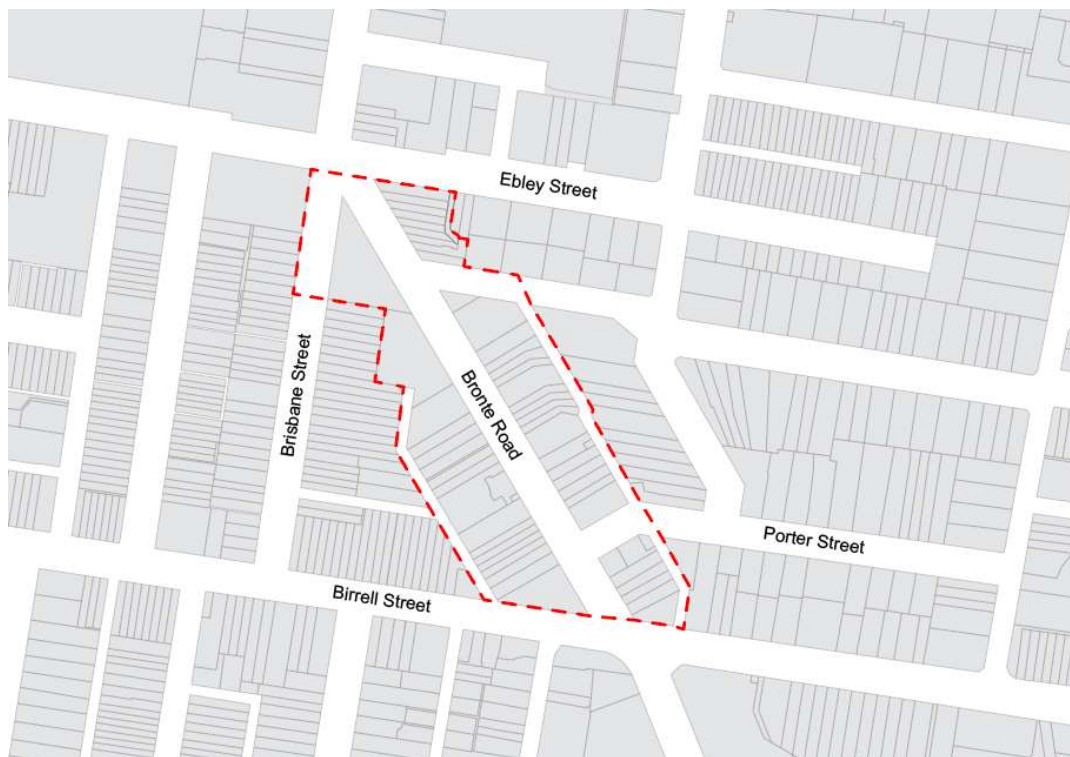


Figure 50 Bronte Road Centre

#### Existing Character and Built Form

The Bronte Road centre forms one key entrance route to the Bondi Junction Strategic Centre. It is broken up by existing residential lots and larger lots with little activity. The centre predominantly hosts bulky good retail services and other local businesses and lacks a cohesive character when compared to other centres within the LGA.

The Bronte Road centre has small pockets of consistency towards the north-east end, with local cafes providing the 'hub' for activity towards Bondi Junction. It's location within the LGA is definitely unique, as it sits adjacent to multiple residential streets with large mature trees and other landscape conservation areas, however the centre itself lacks greenery and adequate public domain treatment.

**Desired Future Character Objectives**

- (a) To enable a diversity of businesses, including commercial and urban services, catering to the needs of the broader community.
- (b) To provide places for the arts, entertainment, and culture.
- (c) To support a high level of pedestrian activity and connectivity within and from the centre to Bondi Junction, Queens Park and Charing Cross.
- (d) To promote well-maintained mid-rise buildings of varying styles which form a consistent boulevard of ground floor shop fronts with setback upper storey residential uses.
- (e) To ensure new buildings provide for a high quality of living with a street frontage that gives comfort to human scale.
- (f) To encourage mid-rise buildings that create a transition between the built form scale of Bondi Junction and Bronte Road in between Ebley Street and Birrell Street.
- (g) To facilitate new housing, commercial opportunities, community facilities and public open space.
- (a)(h) To provide new development on non-heritage sites which maintain a consistent street frontage with adjacent developments, setback any 4<sup>th</sup> level, or follow existing street frontages but change materials to promote fine grain-variety of palette, and consider overshadowing to street and back lanes or buildings.

**3.1.2 Charing Cross****Figure 51** Charing Cross Centre**Existing Character and Built Form**

The Charing Cross local village centre is located along a strip of Bronte Road connecting the eastern beaches, Bondi Junction, Centennial Park and the City.

The centre has a diverse range of local shops and services that support the daily needs of local residents and also workers and visitors who frequent the area. The centre has a "high street" character, supporting the local commercial strip as well a major public transport route to and from the City.

The centre continues to fulfill a valuable social role and meeting place for local residents and for the children attending and travelling to the surrounding schools. The diverse local population also includes aged housing within the centre.

The centre is contained within an existing Heritage Conservation Area, reflecting the high heritage significance of the centre. The area maintains a two (2) storey character, with near- complete rows of highly intact Federation terraces (with continuous lateral pitched roofs) and Victorian terraces (with ornate parapet), interspersed with some examples of two storey Interwar and Art Deco apartments.

These buildings of historic character are all of masonry construction, many with painted plaster render with highly decorative finishes. The buildings are mixed-use and maintain a consistent retail ground floor with residential upper storeys and also support awnings over the entire pavement width.

Narrow passages between terraced groups give access to the rear of properties and laneways. Buildings address Bronte Road and do not address the laneways, although some newer buildings have not maintained this approach to the detriment of the Bronte Road streetscape. Numerous buildings within this area possess elements of, or largely intact, original shop fronts. Many others preserve the original entry configuration (i.e. with inset doorway to one side) reconstructed with contemporary materials.

Important views of historic buildings, available from the public domain, include those of the St Mary's Immaculate Catholic Church and associated buildings (a listed State Heritage group), viewed across the community centre at 280-282 Bronte Road.

Appreciation of the high heritage quality of the building stock of this area is compromised by intrusive suspended power lines and the placement above the line of awnings of other built elements such as advertising structures and air conditioning units.

Buildings are a variety of colours in this area which positively contributes to the character of the area. Where a number of adjoining buildings have been painted the same colour the scale and rhythm of the street has been diminished.

The Eastern Suburbs Legion Club is an important community based use in the centre, although the building is an intrusive element in the existing Conservation Area.

At present the public domain is not particularly well defined, blurring into the adjacent residential areas, particularly at the northern and southern ends of the high street.

#### **Desired Future Character Objectives**

- (a) To limit the scale of redevelopment and infill development at the street edge to match the height of the existing heritage parapet façades and roof lines, with setbacks to further levels where appropriate.
- (b) To ensure that the design of infill development remains consistent with the regular division of frontages, where regular divisions occur.

- (c) To ensure an integrated approach and consistent treatment to the conservation of terrace groups of buildings of historic character.
- (d) To minimise 'visual clutter' through control of peripheral building elements.
- (e) To encourage the conservation of historic architectural details and reconstruction of missing or degraded elements.
- (f) To maintain the continuity of awnings where present.
- (g) To maintain Bronte Road as the primary streetscape in the centre with lanes and side passages as secondary frontages.
- (h) To promote Charing Cross as a destination rather than a thoroughfare, where people visit, stay and enjoy.
- (i) To promote a diversity of uses, independent businesses and retail offerings catering to local needs.
- (j) To provide places for the arts, entertainment and culture.
- (k) To increase urban greening where appropriate.
- (l) To promote a high level of pedestrian activity and connectivity within and from the centre to Bondi Junction, Queens Park and Bronte Beach.
- (m) To maintain a prevailing and consistent streetscape, comprising distinctive and well-maintained low-rise Victorian, Federation and Inter-war buildings and shopfronts, reflective of the historical evolution of Waverley's oldest commercial centre.
- (n) To protect the setting of and views to landmark buildings, including the Bell Towers at St Marys Immaculate Church, that are visible across the LGA aided by the centre's ridgeline topography.
- (o) To ensure development is well designed and responsive to existing built form, history and heritage, with appropriate street frontage heights and upper storey setbacks.
- (p) To ensure any new building, or alterations or additions must respect the HCA and its design characteristics without mimicking heritage detailing.
- (q) To ensure that new development should respect the traditional patterns and proportions of the existing development. Additions should be carefully designed to respect the scale, massing and proportions of the existing building and its key design elements and involve the least amount of alterations to significant fabric
- (r) To ensure that no additions are permitted within the front setback of buildings unless it can be clearly demonstrated that the new structure will not dominate the streetscape and subject building obscure views to the building or adversely impact the cultural significance of the place
- (s) Corner development should accentuate the corner and provide a transition from one street to another.
- (t) Larger building façades should be articulated in a regular rhythm to respond to the late 19th Century and early 20thC street-scape characteristic of the HCA.



**3.1.3 Macpherson Street****Figure 52** Macpherson Street Centre**Existing Character and Built Form**

The Macpherson Street local village centre provides approximately 60 small commercial premises spread out along the length of the street, stretching from Leichhardt Street in the West to St Thomas Street in the East. The range of shops provide for the daily needs of the local community. To the East, near St Thomas Street, the building stock along the ridge-top road is characterised by three (3) storey, mixed-use masonry buildings of diverse styles, both pre-War (c1900) and Interwar. Characteristic buildings have ground floor shops under continuous awnings, residential upper storeys, and some possess intact shop-fronts, or some original elements.

A number of buildings at the west end are higher density residential buildings, while other buildings retain original shop-fronts. Intrusive buildings along Macpherson Street include multi-storey residential and large non-residential buildings. To the West, near Lugar Street, the area is characterised by two (2) storey Interwar commercial buildings of masonry construction, with both decorative face brick and rendered and painted finishes.

Significant views of the ocean exist east along Macpherson Street and to Clovelly looking south from the junction of Macpherson and St Thomas Streets.

Simpson and Macpherson parks are located at the junctions of Macpherson Street with Firth and Carlton streets. These contribute significantly to the character of the centre, allowing clear southern vistas and valued open space. The existing buildings are of two (2) to three (3) storeys in height.

**Desired Future Character Objectives**

- (a) To maintain the built form arising from the historical subdivision pattern and the small shop character at street level.

- (b) Maintain the public views and outlook at the eastern end of the centre, as well as outlook over open space at the western end of the centre.
- (c) To promote the centre as welcoming and inclusive, with a relaxed and casual look and feel.
- (d) To promote a diversity of uses, independent businesses and retail offerings catering to local needs.
- (e) To create and maintain a cohesive and vibrant streetscape, with leafy trees, verge gardens and areas for people to stop and congregate.
- (f) To maintain a physical and visual connection to the coast.
- (g) To maintain low-rise (human-scale) built form of varying styles, with active shopfronts that are open to the public domain.
- (h) To ensure new buildings are of human scale and provide for a high quality of living.
- (i) To maintain fine grain shop-fronts.

### 3.1.4 Bronte Beach



Figure 53 Bronte Beach Centre

#### Existing Character and Built Form

The Bronte Beach Neighbourhood Centre is typified by a single stretch of two (2) to three (3) storey mixed-use, largely Interwar, buildings with retail frontage at ground level under continuous awnings. Upper storeys (that is, storeys above ground level) are used for residential purposes.

Whilst there are street awnings, those to the western end of the strip are less consistent and successful than those to the eastern end. At the western end both glass and canvas



awnings exist which vary from the overall uniform character of the pedestrian experience established at the eastern end.

All buildings of historical character are of brick construction, with painted, rendered, and/or face brick finishes.

Significant views (some partly screened by trees along the bus terminus) of Bronte Beach, Bronte Park and the ocean are possible from all points along this section of Bronte Road.

#### Desired Future Character Objectives

- (a) To maintain the existing scale of the small centre.
- (b) To maintain mixed use developments in the centre, with ground floor local shops and services and upper level residential use.
- (c) To maintain, and where possible in the future, enhance, the range of local shops and services to meet the day to day needs of local residents.
- (d) To provide a diversity of businesses and retail offerings, located in smaller uniform shopfronts, that cater to local and visitor needs.
- (e) To retain the low-rise distinctive heritage and character buildings that frame the street, comprising ground floor business and retail offerings, with residential uses on the upper floors.
- (f) To retain the through-site link from Pacific Street to Bronte Road.
- (g) Protect views to Bronte Beach by reduction of visual clutter created by signage, large public domain fixtures and plantings.



Figure 54 Belgrave Street Centre

#### Existing Character and Built Form

This centre is made up of a collection of four (4) shops located at the intersection of Belgrave and Murray Streets. This area has a modern, post-war suburban character, with few clear historical elements and varied building typology.

The centre is surrounded by predominantly small lot and medium density residential housing. Though the centre is small, it serves the local residential catchment and passing trade from the Bronte Public School (located towards the south). 45 Belgrave Street is a two storey mixed-use, red brick corner shop, while 47-49 Belgrave Street are Federation shops and housing. 'Pocket' parks exist on opposite corners to these buildings providing a landscape feature to the area. The 'shop house' buildings of 47-49 Belgrave Street are of a scale and detail appropriate to the location providing a degree of character to the area.

#### Desired Future Character Objectives

- (a) To maintain the existing scale of the small centre.
- (b) To maintain mixed use developments in the centre, with ground floor local shops and services and upper level residential use.
- (c) To maintain, and where possible in the future, enhance, the range of local shops and services to meet the day to day needs of local residents.
- (d) To promote local business and retail offerings catering to neighbourhood needs.
- (e) To ensure a cohesive and vibrant streetscape, with verge landscaping and a well-maintained community park.
- (f) To encourage upgrades to the facades of existing buildings.

## 3.1.6 Flood Street



Figure 55 Flood Street Centre

**Existing Character and Built Form**

The buildings in this local [village](#) centre occupy a bend in the corner of Old South Head Road. They are of mixed architectural fabric. The immediate environment is dominated by traffic and the commercial/retail use of some of the buildings provides a buffer between the road and the residential buildings that sit behind these uses.

The centre has a number of late Victorian, Federation, and Inter-[wW](#)ar style dwellings with commercial uses being housed within modern structures.

Height varies between one and three storeys, with buildings located to the front of the property boundaries. Buildings are typically of masonry construction, with residential buildings possessing decorative face and painted brick work.

**Desired Future Character Objectives**

- (a) To maintain the [active uses](#) ~~mixed-use character~~ of the centre by way of shops and services at ground level, ~~and residential units above.~~
- (b) To provide an attractive location for small businesses with exposure to Old South Head Road.
- (c) To provide a safe and walkable public domain.
- (d) To retain low-rise distinctive heritage and character buildings that frame the street, comprising ground floor business and retail offerings, with residential uses on the upper floors.

**3.1.7 Curlewis Street****Figure 56** Curlewis Street Centre**Existing Character and Built Form**

This intersection is a prominent marker along the length of Old South Head Road, dominated by traffic and providing little pedestrian amenity.

The buildings are mixed architecturally with no predominant style or built form. The relative importance and scale of the intersection is not reflected in the scale of development.

Buildings are of varied height, yet most contain a retail/commercial ground floor and residential upper storeys, and possess some historic character.

**Desired Future Character Objectives**

- (a) To accommodate a potential increase in the general scale of development in this section of the centre, subject to appropriate site consolidation and satisfying amenity considerations and impacts on adjoining sites.
- (b) To maintain and expand on the current range of land uses, including automotive repairs and service station.
- (c) The site 14-28 Curlewis Street is considered key to the long term objectives of this Part and public domain environment of this precinct. The development of this site to address each of the three (3) street boundaries by building to each property boundary is considered key. The resultant building form will anchor the built form of the intersection while ensuring that each street, Blair and Curlewis streets, is given an improved urban form and scale.

- (d) To create a vibrant streetscape, marking the 'entrance' to Bondi Beach and Waverley LGA, with consistent verge landscaping and signage.
- (e) To promote a diversity of businesses, catering to the needs of the local community.
- (f) To provide a mixture of uses, including night time uses, and to ensure appropriate interfaces between different uses.
- (g) To promote the provision of important urban services that cater to the needs of the broader community.
- (h) To provide increased urban greening through climate appropriate planting, canopy trees and raingardens.
- (i) To promote low to mid-rise buildings of varying styles that frame the street.
- (j) To ensure that new buildings and / or refurbishment of existing buildings are well designed and responsive to existing built form, whilst accommodating a potential increase in scale (where appropriate).
- (k) To grow and consolidate the commercial area within the centre.
- (l) To maintain special features of character buildings at 1-7, 2 and 9 Curlewis Street.
- (d)(m) To ensure that new building and/or the refurbishment of existing buildings are well designed with quality materials, respecting existing built form and character if historical in nature, whilst accommodating a potential increase in scale (where appropriate).

### 3.1.8 Old South Head Road, at Murriverie Road



Figure 57 OSH Rd, at Murriverie Road Centre

#### Existing Character and Built Form

This section of the centre provides a break in the residential streetscape and contains a set of retail/commercial shops with residential uses above. The scale is predominantly two storeys.

The area is also dominated by traffic movement and the commercial strip consists of trade shops and outlets, with no local convenience shops.

This section of the centre contains no heritage items, or heritage conservation area listings. With a varied building typology, including some pre-war items, this area does not possess a uniform or identifiable character.

#### Desired Future Character Objectives

- (a) To maintain the predominantly two storey scale, with any additional levels (if appropriate) being set back from the street edge.
- (b) To maintain and remediate original shop fronts as part of any future development.
- (c) To maintain the mixed-use character of the centre by way of shops and services at ground level and residential units above.
- (d) To provide a clean environment, with waste disposal managed efficiently.
- (e) To ensure that the centre provides universal access to all users.
- (f) To promote low-rise well-maintained buildings, comprising ground floor business and retail offerings.
- (g) To ensure that new buildings and/or refurbishment of existing buildings are well designed and responsive to existing low-rise built form, with appropriate setbacks at upper levels.
- (h) To retain and enhance character buildings through the centre including 369A-371 and 377-381 Old South Head Road.

#### 3.1.9 Rose Bay South



Figure 58 Rose Bay South Centre



### Existing Character and Built Form

The five retail and commercial clusters found along Old South Head Road accommodate approximately 110 shop front premises of which approximately 70 are within Waverley. The two clusters forming the Rose Bay Small Village contain a variety of uses that, when combined with the retail strips on the Woollahra side of the road, provide the daily needs of the local community.

Shop top housing is an important feature of Old South Head Road and provides housing diversity and affordability. This increases pedestrian activity and presence within the village.

### Desired Future Character Objectives

- (a) To ensure an integrated approach and consistent treatment to the conservation of buildings of historic character.
- (b) To maintain and improve the continuity of awnings over the footpath.
- (c) To maintain Old South Head Road as the primary streetscape in the village with side streets as secondary frontages.
- (d) To create a cohesive streetscape, comprising well-designed low-rise buildings of varying styles which form a consistent street frontage with small active shopfronts.
- (e) To ensure a diversity of uses, including commercial, retail, health and residential.
- (f) To maintain a clean environment, with waste disposal managed discreetly and efficiently.
- (g) To ensure that the centre provides universal access to all users.
- (h) To ensure new buildings are well designed and responsive to existing built form, with appropriate street frontage heights, upper storey setbacks and active ground floor uses.
- (i) To ensure that building services and basement car parking do not compromise the active street frontage and business opportunities that the Old South Head Road high-street offers.

#### 3.1.10 Blake Street

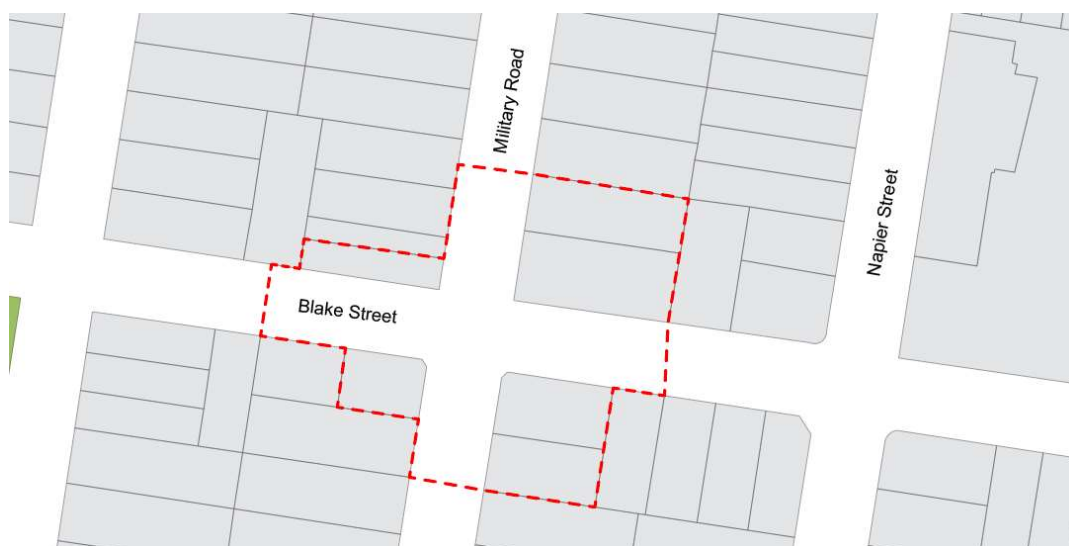


Figure 59 Blake Street Centre



**Existing Character and Built Form**

This local centre has several small shops and commercial uses, servicing the local resident community. The centre does not possess a distinctive historical or neighbourhood character. Modern buildings and renovations having occurred over time creating a varied building typology and street edge definition. While the scale of buildings varies, it is generally two (2) storeys in character. Some buildings in the centre possess street awnings over the footpath.

Due to its elevated location the centre enjoys prominent views west along Blake Street to the inner Harbour and City skyline.

**Desired Future Character Objectives**

- (a) To establish and support a centre characterised by mixed use development incorporating small local shops and services for the local resident community.
- (b) To encourage new mixed use development with ground level local shops and services and upper level residential use.
- (c) Where redevelopment in the neighbourhood centre occurs, to ensure the scale of new development protects the residential amenity of adjoining and surrounding properties.
- (d) To support night-time uses in the centre.
- (e) To better maintain the area so that it is clean and attractive.
- (f) To ensure that the centre provides universal access to all users.
- (g) To ensure new buildings and/or refurbishment of existing buildings are well designed and responsive to existing low-rise built form.
- (h) To retain the low-rise built form of varying styles, with active shopfronts that contain local business and retail offerings catering to local needs.

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**3.1.11 Rose Bay North**



Figure 60 Rose Bay North Centre

#### Existing Character and Built Form

The five retail and commercial clusters found along Old South Head Road accommodate approximately 110 shop front premises of which approximately 70 are within Waverley. The two clusters forming the Rose Bay Small Village contain a variety of uses that, when combined with the retail strips on the Woollahra side of the road, provide the daily needs of the local community.

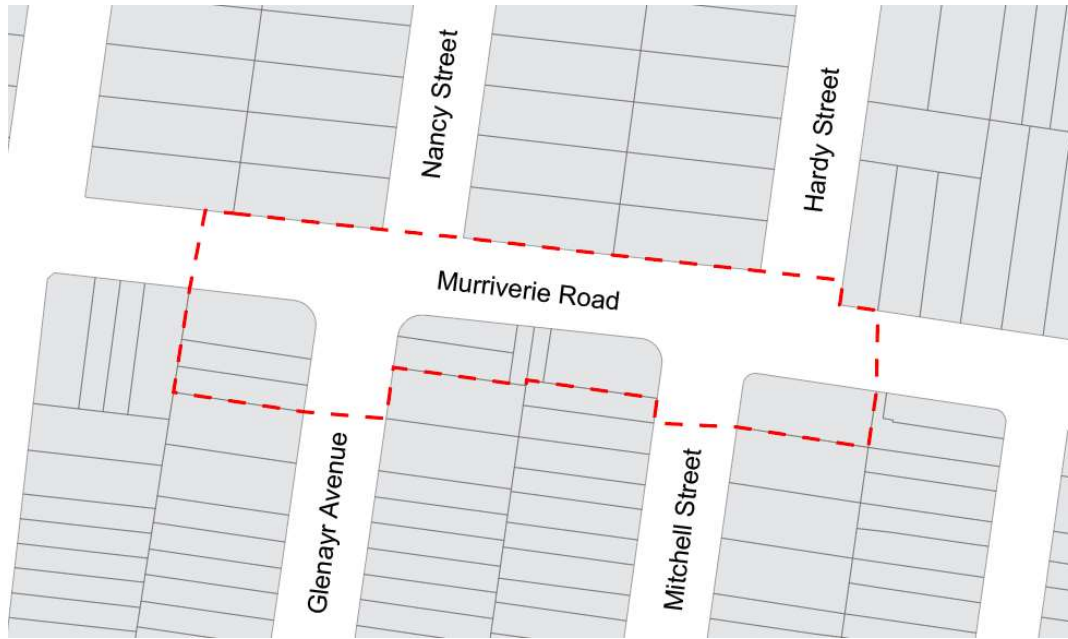
Shop top housing is an important feature of Old South Head Road and provides housing diversity and affordability. This increases pedestrian activity and presence within the village.

#### Desired Future Character Objectives

- (a) To ensure an integrated approach and consistent treatment to the conservation of buildings of historic character.
- (b) To maintain and improve the continuity of awnings over the footpath.
- (c) To maintain Old South Head Road as the primary streetscape in the village with side streets as secondary frontages.
- (d) Maintain a good distinction between the mixed use sections of Old South Head Rd and residential side streets.
- (e) To promote a diversity of uses, businesses and retail offerings, that cater to local needs.
- (f) To maintain a clean and attractive environment, with waste disposal managed discreetly and efficiently.
- (g) To ensure that the centre provides universal access to all users.
- (h) To promote well-maintained mid-rise buildings of varying styles which form a consistent street frontage of ground floor shop fronts and upper storey residential.

- (i) To ensure new buildings are well designed and responsive to existing built form with appropriate street frontage heights.
- (j) To maintain and encourage a diverse range of shops and services.

### 3.1.12 Murriverie Road



**Figure 61** Murriverie Road Centre

#### Existing Character and Built Form

The Murriverie Road local village centre is comprised of approximately nine shops which are used for commercial and retail purposes with some residential uses above ground floor.

The shops are spread over three sections of the street, providing a range of goods and services to assist in meeting the daily needs of the local residents. The area maintains a strong two (2) storey character.

The mixed use buildings are of a range of typologies, built to the street edge with awnings. The buildings of historic character at No.1 Mitchell Street have had some alterations, although the original shop front exists.

The strongest historic character of the centre is provided by the substation, located east of the pocket park on Murriverie Road.

No. 2 Mitchell Street is a good example of a successful transitional building in terms of height (from one (1) to two (2) storeys) and function (from mixed-use to residential).

The centre accommodates a small pocket park at the south east intersection of Murriverie Road and Glenayr Avenue.

#### Desired Future Character Objectives

- (a) To ensure appropriate architectural design and scale for corner site development.
- (b) To maintain and enhance accessibility to public open space.
- (c) To promote a cluster of vibrant independent businesses and retail offerings, catering to local needs.
- (d) To provide a clean environment, with waste disposal managed efficiently.
- (e) To ensure that the centre provides universal access to all users.
- (f) To retain low-rise distinctive heritage and character buildings that frame the street.
- (g) Maintain fine grain shop-fronts and preserve existing character of heritage or character buildings.
- (h) Promote activation of the streetscape.
- (i) Maintain fine grain shop-fronts and preserve existing character of heritage or character buildings.

### 3.1.13 Vaucluse

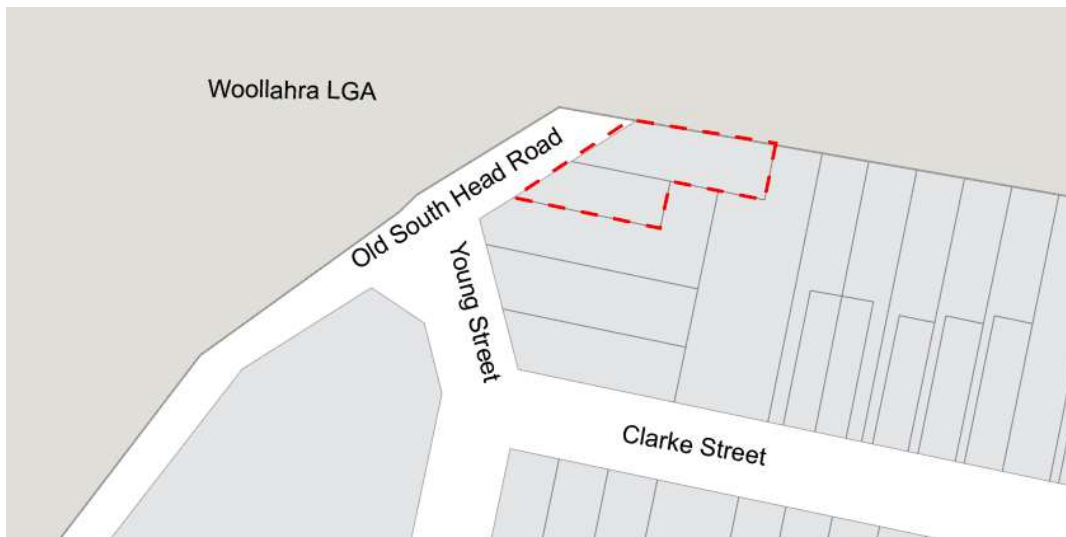


Figure 62 Vaucluse Centre

#### Existing Character and Built Form

The Vaucluse Centre services the northern-most portion of the Waverley LGA. Two lots feature shop top housing development, with ground floor uses activating the immediate surrounds. The vitality of this centre is largely driven by the individual operators of the centre.

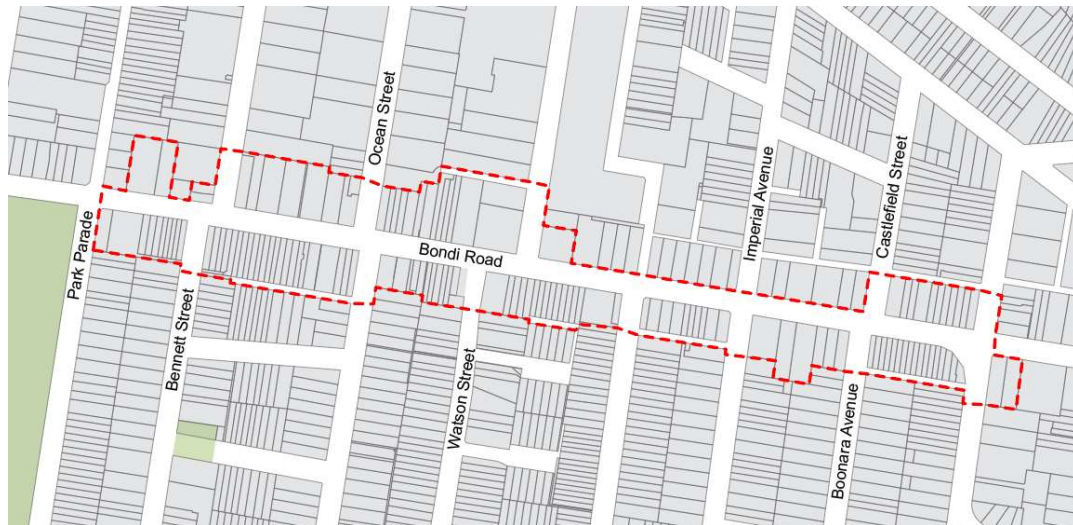
The centre often has people seated outdoors, with the close proximity of the centre to public open space attracting good local business.

#### Desired Future Character Objectives

- (a) To promote a safe and attractive meeting point for locals and visitors alike.

- (b) To promote a cohesive and vibrant streetscape, with leafy trees, verge gardens and areas for people to stop and congregate.
- (c) To ensure that the centre provides universal access to all users.
- (d) To retain and maintain the small cluster of shop-top housing.

### 3.1.14 Bondi Road



**Figure 63** Bondi Road Centre

#### Existing Character and Built Form

Bondi Road is an important and busy transport corridor that runs along the 'spine' connecting Bondi Beach to Bondi Junction and the City. The existence of numerous bus stops draw people to Bondi Road, increasing pedestrian presence. A strength of the village is good pedestrian accessibility to the retail shops from a relatively large residential catchment.

The strip is composed of smaller 'shop house' buildings of a scale which are reflective of the historical small scale lot subdivision pattern. Buildings are generally two (2) storeys to the street edge, having an effective height, due to the existence of parapets and roof forms, of three (3) levels.

A number of contradictions exist in the form of larger high rise 1960's and 1970's residential and hotel towers. These buildings are inconsistent with the overall scale of the street fabric. Lower podium levels of the building (lower two (2) to three (3) levels) tend to have front and side setbacks inconsistent with adjoining development and the rhythm of the streetscape. All existing buildings of historic character are mixed use, with commercial ground floor and residential upper storey(s).

Numerous buildings within this area possess elements of, or largely intact, original shop-fronts. Many other buildings preserve the original entry configuration (i.e. with inset doorway to one side) reconstructed with contemporary materials.

The Bondi Road centre comprises of three distinct existing characters: West Bondi Road, Central Bondi Road and East Bondi Road. A distinct character exists between the western end and eastern end of Bondi Road, both in built form and the public domain.

The western end of Bondi Road, closer to Bondi Junction, is run down and has a less consistent character when compared to the eastern end. The western end has more diverse land use offerings, with a mix of retail, hospitality and personal services. The built form is inconsistent with a former service station, a range of residential and shop-top housing buildings, and public buildings such as St Patrick's Catholic Church and the Waverley Woollahra Arts School.

The northern side of Central Bondi Road section is largely residential development with a few health & beauty services. The southern side is mostly shops, with two residential flat buildings near Boonara Ave. The residential developments along this section of Bondi Rd distinguish this section from the West and East Bondi Rd characters.

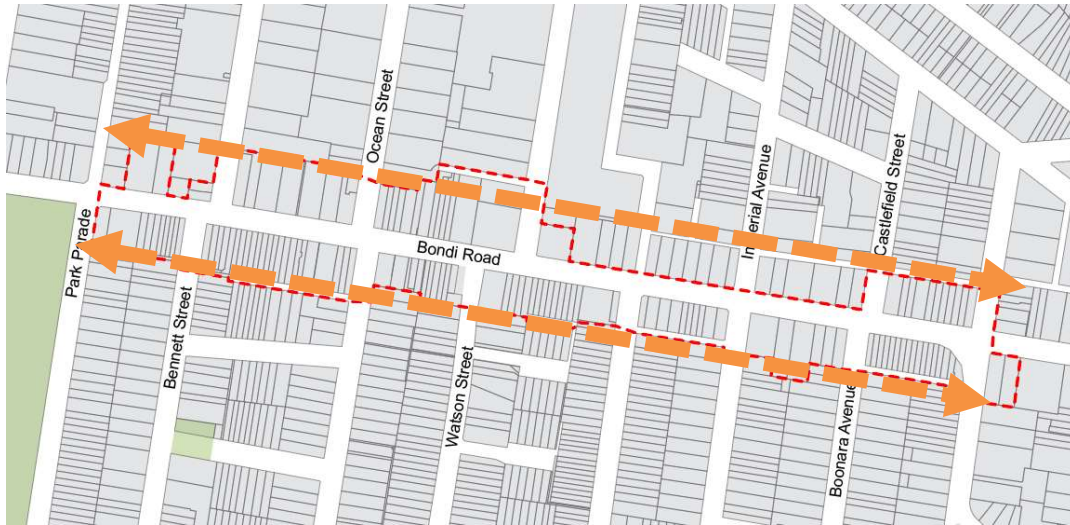
The eastern end of the corridor is lively and bustling with activity in the evenings, as a local and tourist 'go-to' for restaurants and recreation. The eastern end of Bondi Road, closer to Bondi Beach, has a mix of commercial and residential ground floor uses. The commercial uses are typically at the street frontage, whilst the blocks of apartments have large front setbacks with vehicle crossings and landscaped areas. There are a number of inter-war period 3-4 storey walk-ups and shop-top housing buildings that provide a strong sense of character to the area. The ground floor commercial uses are mostly hospitality and retail and have created a retail 'hub' around the intersection of Denham Street and Bondi Road, due to the fine grain nature of the shop fronts and human scale of the built form.

#### **Desired Future Character Objectives**

- (a) To maintain the role and character of Bondi Road in providing local shops, services and residential accommodation for the local community.
- (b) To limit the scale of redevelopment and infill development at the street edge to match the parapet façade height of buildings of historic character, with setbacks to further levels where appropriate.
- (c) In the case of future works and improvements to the 1960s and 1970s residential and hotel towers that exist along Bondi Road, to encourage the street and podium levels to better knit the street fabric together through the introduction of shop fronts at ground level.
- (d) To encourage the use of rear courtyards during trading hours to extend business operations where there will be minimal adverse impacts upon surrounding neighbours.
- (e) To support shared cycle/pedestrian links parallel to Bondi Road that will connect Bondi Junction and Bondi Beach, increase pedestrian and cycle safety, provide rear lane activation opportunity and improve service access and on-site parking arrangements.
- (f) To promote Bondi Road as an important local centre that provides a walkable range of goods and services to the surrounding residential community.
- (g) To retain a diversity of independent uses, businesses and retail offerings catering to local needs.
- (h) To ensure ground floor premises provide active and inviting street frontages.
- (i) Minimise residential development within the centre to ensure retention of employment opportunities.
- (j) To ensure development incorporates best practice sustainability initiatives.
- (k) To promote localised energy generation including through solar panels and microgrids.
- (l) To promote green roofing and increased planting on buildings where appropriate.
- (m) To promote a clean environment, with waste disposal managed discreetly and efficiently.



- (n) To encourage heat-reflective materials and increased shading to create a cooler climate for pedestrians.
- (o) To ensure that the centre provides universal access to all users.
- (p) To retain the distinctive historic urban fabric of the high street, including the fine grain shopfront pattern, two-storey street frontage and nil-setbacks, and the architectural detail of the original facades.
- (q) To protect and celebrate the historic character and diverse buildings along Bondi Road.
- (r) To ensure infill development is well designed and responsive to the existing built form and scale, including heritage and character buildings, and is of human scale.



**Figure 64** Indicative representation of shared cycle/pedestrian links parallel to Bondi Road

### 3.1.15 Fletcher Street





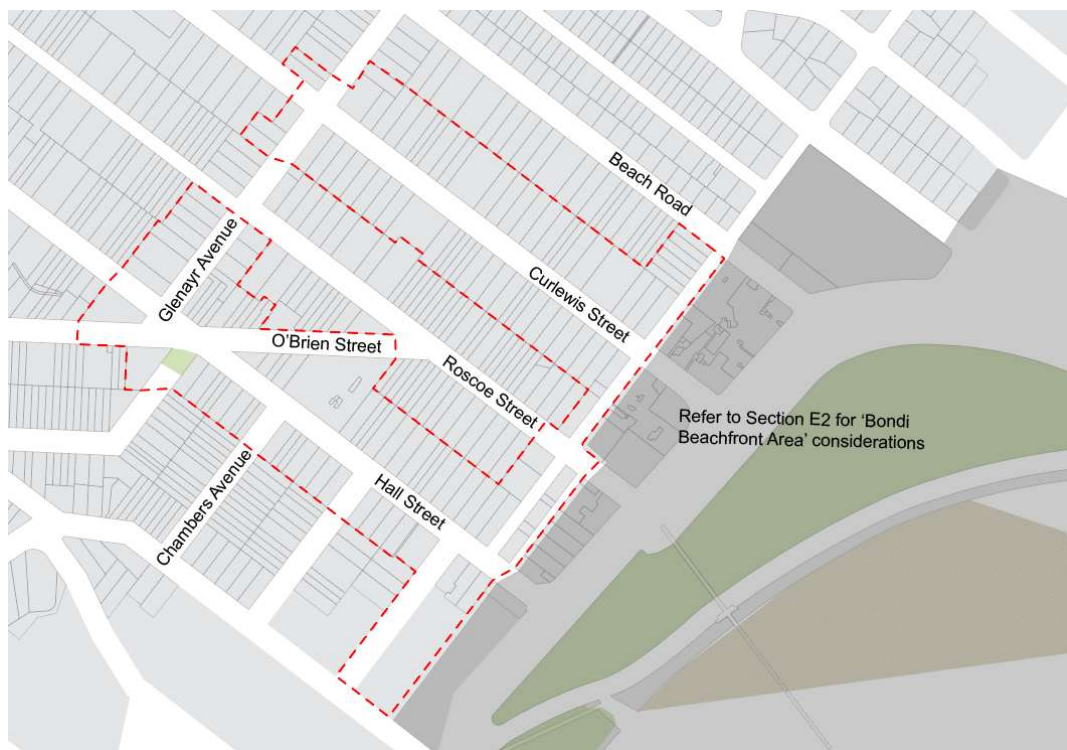
Figure 65 Fletcher Street Centre

#### Existing Character and Built Form

The Fletcher Street Centre contributes to the neighbourhood feel of the area. The centre has a medium capacity to support resilience. The nearby reserve provides a place to sit and enjoy a coffee in the sun. The centre receives a good amount of foot traffic due to its location on the way to Tamarama Beach.

#### Desired Future Character Objectives

- (a) To support a cluster of vibrant independent businesses and retail offerings catering to local needs, located within distinctive low-rise character buildings.
- (b) To ensure that the centre provides universal access to all users.
- (c) To ensure the refurbishment of existing buildings maintains the heritage character of the centre.
- (d) To enhance character buildings through art and planting.

**3.1.16 Bondi Beach****Figure 66** Bondi Beach Centre

Refer to Part E2 of this Chapter for controls relating to the Bondi Beachfront Area.

**Existing Character & Built Form**

The character of Bondi Beach centre is diverse, casual, approachable, and friendly. The built form is varied with many original buildings remaining as well as newer infill development.

Hall Street and the southern end of Glenayr Avenue contain predominantly mixed use development, with retail shops at ground floor level and residential uses on the upper floors. The retail strip is also adjoined by residential streets along its length, resulting in a vibrant mixed use area. Challenges exist however in terms of managing the interface between the non- residential and residential uses.

Due to the popularity of Bondi Beach and Hall Street, the extent of regional and local traffic and car parking has a strong influence on the character and use of the area, particularly the public domain.

The area contains a consistent pattern of retail buildings located to the front edge of the street boundaries, although some have substantial setbacks from street boundaries. Buildings are typically of masonry construction, with face (decorative) brick and/or painted brick.

In terms of building footprint, regular side passages tend to emphasise separated, regular lots of narrow frontage.

Within the Hall Street precinct are two key sites, namely the Bondi Post Office on the corner of Hall Street and Jacques Avenue, and the intersection of Hall Street, O'Brien Street and Glenayr Avenue.

### Desired Future Character Objectives

- (a) To maintain Hall Street and the southern end of Glenayr Avenue as a separate and discrete precinct within the wider Bondi Beach town centre, with the role and character of providing local shops, services and residential accommodation for the local community.
- (b) To effectively manage the retail/commercial and residential interface in the centre.
- (c) To maintain and enhance accessibility to public open space.
  - (d) To promote a diversity of uses, businesses and retail offerings in smaller shopfronts with active frontages to maximise interactions and interest.
  - (e) To consistently maintain the public realm to ensure that it is green, clean and free of litter.
  - (f) To increase urban greening around Bondi Beach.
  - (g) To promote places for the arts, entertainment and culture as well as health and fitness.
  - (h) To retain a diversity of independent uses, businesses and retail offerings catering to local needs.
  - (i) To ensure ground floor premises provide active and inviting street frontages.
  - (j) Minimise residential development within the centre to ensure retention of employment opportunities.
  - (k) To ensure development incorporates best practice sustainability initiatives.
  - (l) To promote localised energy generation including through solar panels and microgrids.
  - (m) To promote a clean environment, with waste disposal managed discreetly and efficiently.
  - (n) To protect and promote open spaces and corridors providing visual and physical connection through to Bondi Beach.
  - (o) To encourage heat-reflective materials and increased shading to create a cooler climate for pedestrians.
  - (p) To balance the shared use of the public domain between pedestrian movements, landscaping, outdoor dining and vehicle access and parking.
  - (q) To advocate for improved public transport capacity and services.
  - (r) To ensure that the centre provides universal access to all users.
  - (s) To ensure infill development is well designed and responsive to existing built form, history and heritage, with appropriate street frontage heights and upper storey setbacks.
  - (t) To promote a mix of old and new buildings, with adaptive re-use of heritage and encouragement of innovative modern design for new development.
  - (u) To retain the distinctive historic urban fabric including the fine grain shopfront pattern.
  - (v) To protect and celebrate the historic character throughout the centre.
  - (w) To encourage entertainment or event uses, where residential amenity can be reasonably retained.
  - (x) To maintain fine grain shop-fronts in new developments, particularly along Hall Street, Glenayr Avenue and Gould Street.

## 3.1.17 Seven Ways



Figure 67 Seven Ways Centre

**Existing Character and Built Form**

The centre comprises two areas. The first is a small group of buildings located at the corner of Curlewis Street characterised by two (2) storey Interwar mixed-use buildings. The second area is centred around the 'Seven Ways' intersection which is largely comprised of two (2) to three (3) storey Interwar apartment buildings and also notable for its mixed use buildings with ground floor shops and residential storeys above.

In addition to the two distinct centres, Glenayr Avenue includes a series of small scale nodes, interspersed with residential development.

The 'Seven Ways' commercial centre has good quality local shops and cafes. Three of the buildings addressing the 'Seven Ways' and 83-85 Glenayr Avenue possess intact original shop-fronts.

Several mixed-use corner sites were (commercial ground floor and residential upper floors) assessed as being buildings of historic character. They represent examples of successful transition in form and function between the commercial uses of Glenayr Avenue and the residential character of the side streets. These corner buildings enable a transition by a reduction in height, and the incorporation of a setback, in those (northern) facades while addressing the residential side street.

**Desired Future Character Objectives**

- (a) To maintain the role and character of the discrete sections that make up the Glenayr Avenue centre, including the provision of local shops, services and residential accommodation for the local community.
- (b) To effectively manage the retail/commercial and residential interface in the centre, and in particular maintain the strong residential character where it currently exists along Glenayr Avenue.
- (c) To ensure an appropriate architectural design and scale for corner site development.
- (d) The 'Seven Ways' intersection of Blair Street and Glenayr Avenue should stand as the focus of the Glenayr Avenue precinct. This intersection has the potential to be an even more vibrant and active public space.
- (e) To support diverse uses, businesses and retail offerings, interspersed by residential and civic uses and book-ended by open and active community spaces.
- (f) To maintain a fine-grain streetscape with well-maintained and distinctive character buildings.
- (g) Landscaping and tree planting that provides continuous greenery through the centre.
- (h) To promote a clean environment, with waste disposal managed discreetly and efficiently.
- (i) To encourage a high level of pedestrian and cyclist activity and connectivity within and from the centre to surrounding centres and Bondi Beach.
- (j) To ensure that the centre provides universal access to all users.
- (k) A fine-grain streetscape, with well-maintained and distinctive character buildings.

**3.1.18 North Bondi**



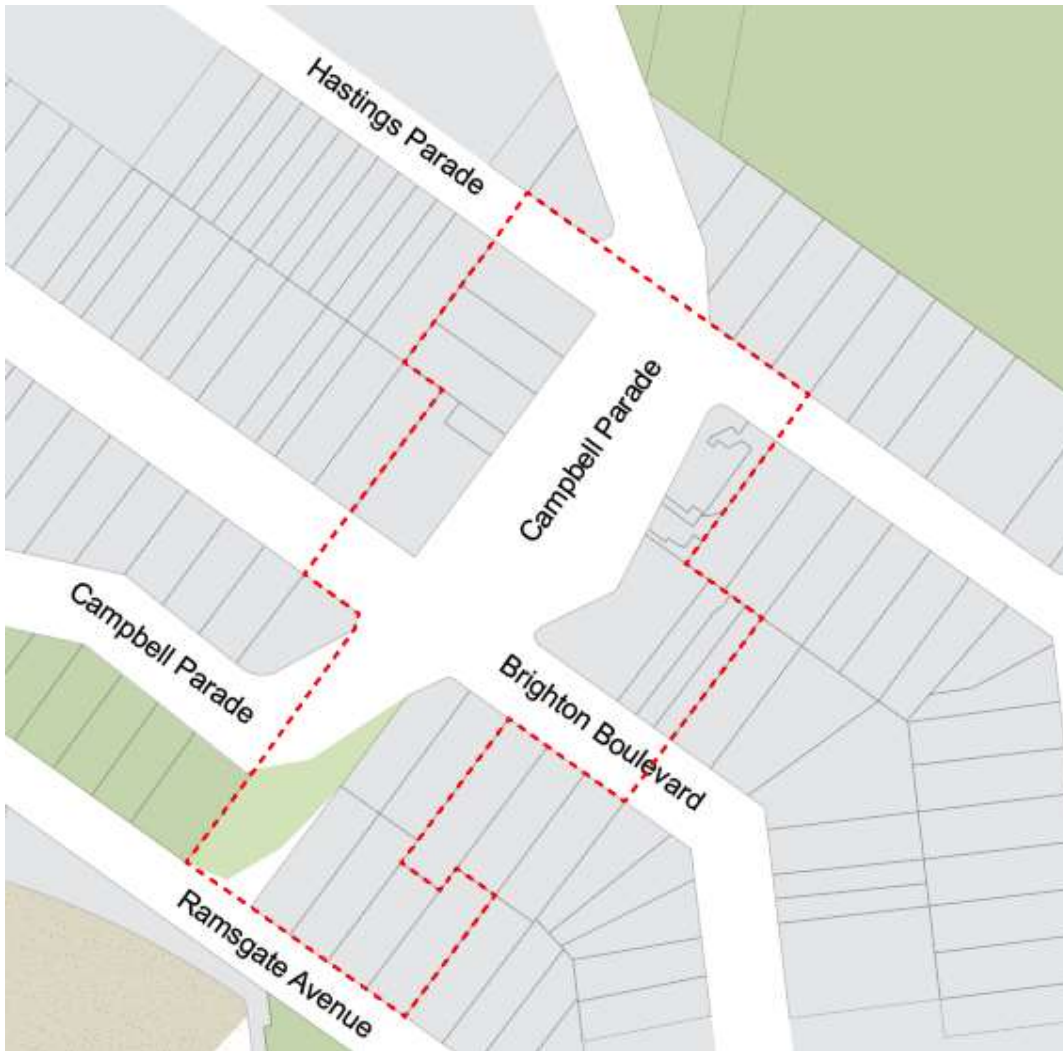


Figure 68 North Bondi Centre

#### Existing Character and Built Form

North Bondi Neighbourhood Centre contains a cluster of shops adjacent to the bus terminus. It exists at the northern end of Campbell Parade where it meets Scarborough Crescent, at the intersection with Brighton Boulevard.

The Campbell Parade/Terminus local shopping strip offers a range of retail and other services, providing for the daily needs of the local residents.

This area has a varied building typology, although building styles are all of the Interwar period and built to the street property boundaries. Construction does not exceed three (3) storeys (generally two (2) storeys with a pitched roof) and the majority of buildings of historic character are of brick construction with decorative face brickwork.

All of the buildings of historic character are mixed use, with commercial ground floor and residential upper storeys.

Most buildings in this part of the centre address the terminus/junction area along Campbell Parade and this space is considered to have historic character for the area, given its socially important role as a transport interchange.

**Desired Future Character Objectives**

- (a) To maintain North Bondi as a separate and distinct precinct to the larger Bondi Beach precinct, with the role and character of providing local shops, services and residential accommodation for the local community.
- (b) Where redevelopment in the neighbourhood centre occurs, to ensure the scale of new development protects the residential amenity of adjoining and surrounding properties.
- (c) To maintain the predominantly two - three storey scale of development, at the same time as protecting the existing amenity of properties adjoining the centre.
- (d) The North Bondi RSL is an important community building and considered to be a key site in the centre. It is unlikely that this building marked \*, will ever be developed to conform with the planning controls.
- (e) The bus interchange is a key community site and future development at this site is addressed in the Local Village Centres Public Domain Improvement Plan.
- (f) To retain a cluster of vibrant independent businesses and retail offerings, catering to local needs.
- (g) To provide a visual connection to Bondi Beach.
- (h) To provide clean environment, with waste disposal managed efficiently.
- (i) To promote a safe and walkable public domain that promotes connectivity within the centre and to Bondi Beach and surrounding residential areas.
- (j) To ensure that the centre provides universal access to all users.
- (k) To retain low-rise distinctive heritage and character buildings that frame the street, comprising ground floor business and retail offerings, with residential uses on the upper floors.
- (l) To ensure new building and/or refurbishment of existing buildings are well designed and responsive to existing low-rise built form, with appropriate setbacks at upper levels, and driveway crossovers.
- (m) To ensure that no additions are permitted within the front setback of buildings unless it can be clearly demonstrated that; the new structure will not dominate the streetscape and subject building, will not obscure views to the building, and will not adversely impact the cultural significance of the place.

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**3.1.19 Wairoa Avenue**





Figure 69 Wairoa Avenue Centre

#### Existing Character and Built Form

The Wairoa Avenue Centre contains fine grain shop fronts with a continuous awning characterising the area. The centre is located in a residential area within walking distance to schools, other larger centres, and Bondi Beach and the coastline.

#### Desired Future Character Objectives

- (a) To promote a small clusters of businesses and retail offerings, interspersed by civic and residential uses.
- (b) To provide a clean environment, with waste disposal managed efficiently.
- (c) To increase infiltration opportunities through WSUD.
- (d) To ensure that the centre provides universal access to all users.
- (e) To ensure new buildings are well designed and responsive to the existing built form and scale, including heritage and character buildings, and is of human scale and provides for a high quality of living.
- (f) To ensure low-rise distinctive heritage and character buildings that frame the street.

### 3.2 GENERIC CONTROLS

This section outlines the general planning controls that apply to all centres.

**Note:** Compliance with a control does not guarantee that the objectives are satisfied.

In some instances the design solutions may not be appropriate for the particular site or situation and Council may require an alternative design solution.

In order to ensure the physical characteristics of the site and the nature and proximity of adjoining and nearby development has been considered, a centre analysis is required to be submitted with all development applications which includes the existing built form within the surrounding local village area. Refer to the *Waverley Development Application Guide* for further details.

Annexures are provided to illustrate examples of typical built form envelopes for 2, 3 and 4 storey local village centres as follows:

- *Annexure E3-1 – 2 storeys*
- *Annexure E3-2 – 3 storeys*
- *Annexure E3-3 – 4 storeys*

### 3.2.1 Land Uses

#### Objectives

- (a) To provide for a range of predominately small shops and services to meet the daily needs of the local resident community.
- (b) To ensure the ground floor small shop character of each centre prevails and is protected.
- (c) To limit and manage potentially disruptive uses, such as cafes and restaurants in order that they do not dominate a centre or limit the provision of a broad range of local shops that are needed to meet the needs of the local resident community.
- (d) To promote mixed-use development incorporating high quality residential use above ground level.
- (e) To improve the quality of the built and pedestrian environment, particularly the interface between properties and land uses.

#### Controls

- (a) The ground floor component of a mixed use building is to be used for a permitted non-residential use, with the exception of:
  - (i) Access areas for residential dwellings on upper levels.
  - (ii) Existing purpose built approved and occupied residential dwellings occupying the ground floor of a building.
  - (iii) Where a site addresses a rear lane, the residential dwellings may address the rear lane at ground level but only where all other Local Village Centre planning controls have been satisfied.
- (b) Cafes and restaurants located in corner buildings, with side street frontage to residential streets are to orient the trade area, including any outdoor dining, to the commercial street.
- (c) Seating for cafes and restaurants is to be limited to the enclosed ground floor and, where appropriate, the footpath frontage of buildings.
- (d) Building floors above ground and first floor are to be designed for permanent residential use only.
- (e) Car parking is to be located at basement level with vehicular access from side streets or rear lanes rather than the primary street frontage.
- (f) Vehicle access across the primary street frontage within a Local Village Centre is not supported.

- (g) In cases where no side street or rear lane access is available, development is not required to provide car parking.
- (f) Residential and low scale commercial office uses are acceptable at first floor level.
- (g) Commercial office uses may only take place where the building has been specifically designed, or acceptably adapted, for this use, including adequate separation from residential uses elsewhere in the building.
- (h) Clearly separate and distinguish commercial and residential entries and vertical separation.

### 3.2.2 Public Domain Interface

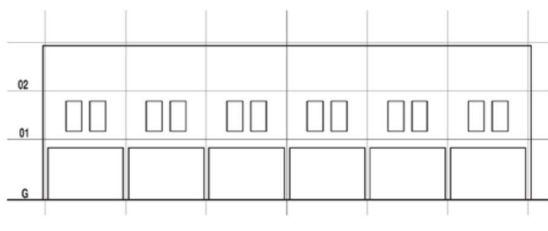
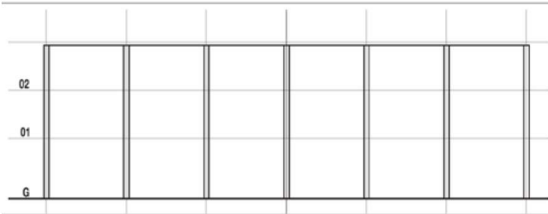
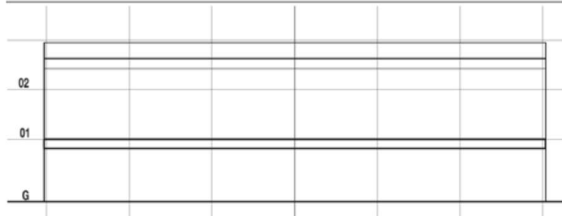
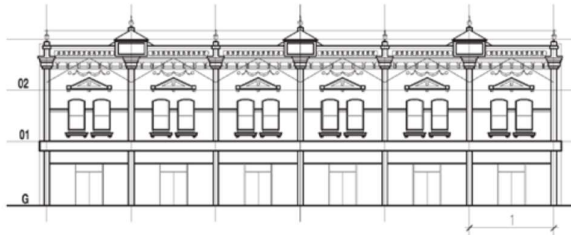
#### Objectives

- (a) To create well defined Local Village Centres, designed for retail trading, appropriate commercial uses and community activity at street level.
- (b) To ensure that ground level frontage is of retail uses to the street edge.
- (c) To ensure interest and vitality by maintaining and encouraging a mix of predominately small scale individual retail outlets.
- (d) To ensure original shop fronts, where they exist, are retained and restored.

#### Controls

- (a) Development must be sensitive to the streetscape character and views. A streetscape and context analysis is to be provided in accordance with *Part B12 Design Excellence*.
- (b) Development on a lot identified in this Part is to provide active street frontages. Refer to *Part B16.2 Active Street Frontages*.
- (c) Buildings are to be located to the front street alignment, with the exception of recommended upper level setbacks, nominated in the controls for each of the individual centres.
- (d) Where existing buildings are setback from the street and are to be refurbished, they are to be extended to the street edge at ground level, except listed heritage items and buildings of historic character.
- (e) Individual buildings are to have a clear street address where entries to upper levels are well defined at the ground floor address.
- (f) New shop fronts are to be consistent in width and height with the predominant and historical character of the street.
- (g) Shop fronts may include recessed entries and display windows, where these are included to provide useable display space and achieve the desired future character of the centre.
- (h) Shop fronts are to be made up predominantly of clear glazing with sill heights to be a maximum of 700mm above finished footpath level along street frontages.
- (i) Access to residential dwellings above ground level should not occupy more than 20% of the principal street frontage of any development.
- (j) There are to be no solid facades along the primary street frontage at ground level.
- (k) Vehicular entries into buildings are not permitted along the primary commercial street frontage of sites, except where contemplated in the planning controls for individual centres.
- (l) The public domain interface of development should provide universal access to all users.

(k)(m) The design of a development proposal is to have regard to the existing streetscape pattern by applying (i) to (v) below.

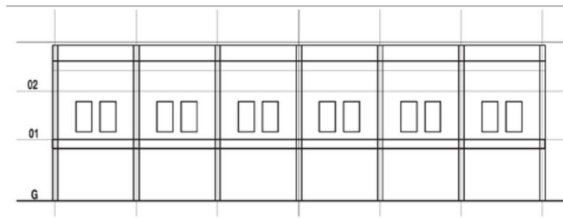


(i) Existing streetscapes are to be analysed to understand the existing streetscape pattern. The pattern can be quantified simply by a height to width ratio. New buildings inserted into an existing streetscape should display similar aspect ratios. This ensures the overall pattern and rhythm of the strip is not negatively impacted by new infill development.

(ii) Horizontal datum points should be established.

(iii) The vertical divisions suggestive of lot subdivision should be referenced even if the development site is larger than the traditional lot sizes.

(iv) Older buildings display a solid to void ratio consistent with a glazed ground level and a more enclosed upper level. The upper levels of these buildings present as a single form with 'punched' openings generally in a masonry background. While a strict replication of this building form is not necessary any new buildings should display similar characteristics in regards to proportions and ratios.



- (v) The application of (i) to (v) above means that a pattern indicating an understanding of the existing streetscape building form can be quickly established so as to guide the direction of new infill development.

### 3.2.3 Built Form

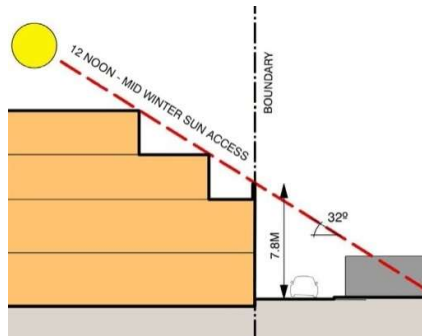
#### Objectives

- (a) To ensure new and refurbished buildings are of an appropriate scale and design quality, achieving the desired future character of each of the centres.
- (b) To ensure development conserves and enhances buildings and locations of historic character.
- (c) To allow, in some locations identified as appropriate in individual centres, some increase in the height and scale of new development, in order to achieve the desired future character for the individual centre.
- (d) To ensure that buildings provide high quality internal environments for the occupants and users of the buildings.
- (e) In the case of development adjacent to buildings of historic character, to promote a complementary scale and form that enhances the character of the centre.
- (f) In the case of corner buildings, to encourage massing and articulation in order to achieve the desired future character of individual centres.
- (g) To ensure good solar access and amenity to the public domain within the individual centres.
- (h) To support excellence in contemporary design.
- (i) To maintain reasonable solar access to residential properties backing onto rear lanes across from village centres.

#### Controls

- (a) Development is to be consistent with the planning controls relating to overall height, floor to ceiling heights and setbacks, outlined for each of the centres in *Annexures E3-1 to E3-3*.
- (b) Ground floor retail depth must allow for adequate display and sales area as well as essential back-of-house storage and loading facilities. In total this must be a minimum of 8 - 10m in depth.
- (c) The preferred building depth for floors above ground level is 10-14m. The maximum building depth for floors above ground level, glazing line to glazing line is 18m. Refer to the control diagrams for each individual centre.
- ~~(d)~~ Sites in local village centres that adjoin residential development at the rear are to provide deep soil zones within the rear setback area with a minimum depth of 2 metres from the boundary.
- ~~(d)~~(e) Ensure any alterations or additions are well designed and responsive to existing built form, history and heritage with appropriate street frontage heights and upper storey setbacks.
- ~~(e)~~(f) The maximum street wall height of buildings fronting rear lanes is 7.8m or two storeys, whichever is the lesser (refer to Figure 56).
- ~~(f)~~(g) Floors fronting lanes which are located 7.8m above the level of the lane or higher (except those on the south side of the lane) and have residential properties

backing onto the rear lane opposite must be setback at an angle of 32 degrees in accordance with Figure 70.



**Figure 70** Setbacks at rear lanes to ensure solar access to neighbours

### 3.2.4 Building Façade Articulation

#### Objectives

- (a) To ensure that buildings are designed and detailed to provide a strong street address, enhance the streetscape and achieve the desired future character of the relevant centre.
- (b) To reinforce the prevailing street pattern and rectilinear building forms as well as predominantly vertical proportion of bays, openings and windows.
- (c) To maintain and promote the vertical emphasis of the narrow built forms.
- (d) To actively support excellence in contemporary design, respecting buildings of historic character with contemporary infill development which does not mimic but builds on the principles of the structure of the streetscape pattern.
- (e) To ensure ground level building frontages are active, open and inviting.
- (f) To reinforce the historic street and subdivision pattern and building articulation to ensure that the rhythm of older street patterns is maintained and enhanced.
- (g) To ensure that, where the amalgamation of sites occurs to achieve a singular larger development area, the rhythmic pedestrian street experience is not lost.

#### Controls

- (a) New buildings should display proportions which respect and build upon proportions similar to the adjoining streetscape and building forms.
- (b) New buildings should draw on the predominant pattern of the existing streetscape. They are to be open and glazed at the street level, have an emphasis toward a singular more enclosed building form at the upper levels and be capped by a lighter more articulated element.
- (c) Balconies to the street facade are to be recessed behind the principal building facade.
- (d) Balustrades to balconies fronting the street are to be predominantly solid with minimal or no glass.
- (e) Development directly adjoining buildings of historic character are to be designed so as to respect the hierarchy of the adjoining facade articulation.

### 3.2.5 Buildings of Historic Character

#### Objectives

- (a) To protect and maintain the historical identity of each of the individual local centres.
- (b) To protect individual buildings that are considered to be of historic character in each of the centres.
- (c) To encourage the ongoing and adaptive re-use of buildings of historic character.
- (d) To allow for new development in the individual centres that complements the character and scale of buildings of historic character.

#### Controls

- (a) Identified buildings of historic character, as detailed in the planning controls for each of the individual local centres, are to be retained.
- (b) Where the building form, detailing or use of individual buildings of historic character have been inappropriately altered and changed, any application to upgrade or re-use the buildings must clearly demonstrate that the architectural and streetscape value of the building will be enhanced by the proposal.
- (c) Any application to demolish an identified building of historic character must clearly demonstrate that a replacement building will possess equal or higher quality contributory value with respect to streetscape, character, architectural design, material quality and construction.
- (d) New development adjacent to buildings of historic character must be sympathetic in scale, alignment, detailing and materials.

### 3.2.6 Building Services and Site Facilities

Building services and site facilities for the purposes of this Part relate to:

- Garbage and recycling collection and storage areas;
- Basement storage areas;
- Mail boxes;
- Laundry facilities; and
- Clothes drying areas.

#### Objectives

- (a) To ensure that adequate provision is made for essential building services and facilities on site, integrated into the overall design and planning of the building.
- (b) To ensure that the services and facilities are unobtrusive and do not detrimentally impact on the appearance of the buildings or the view of the buildings from the public domain or adjoining residential properties.
- (c) To ensure that the use and operation of the building services and facilities does not unacceptably impact on the residential amenity of adjoining residential properties.
- ~~(c)~~(d) To provide a clean environment, with waste disposal managed efficiently.

#### Controls



- (a) Garbage and recycling storage and collection areas, and the structures in which they are contained, are not to be visible from the public domain.
- (b) Setbacks on ground level at the rear are not to be used at all for any purposes associated with storage of waste or recycling material, such as garbage rooms or bottle storage. Buildings are to be designed and used in a manner that ensures that these activities are wholly contained within the building proper. The only exception is for the regular collection of waste and recycling from the rear, in the event of rear lane access. Where a setback at the rear at ground level is provided, it is to be designed and maintained as a landscaped buffer between the subject site and the adjoining properties to the rear.
- (c) The rear of buildings, at ground level, where they back directly on to residential properties or uses, are to be designed to be effectively 'sealed' at the rear, in order that noise and odour transmission from the rear of these premises does not occur in any form that detracts from the amenity of the adjoining residential properties.
- (d) New and refurbished buildings must incorporate venting from ground floor premises in a way that does not result in the transfer of cooking odours impacting on residential properties within the same site/building or neighbouring and adjacent residential properties.
- (e) Air-conditioning units, exhaust fluing, mechanical ventilation ducting, including venting and exhaust structures and equipment associated with ground floor food premises such as cafes and restaurants and the like, are not to be located in front of the front building line or in places clearly visible to the main street frontage or any adjoining or nearby residential properties should be integrated into the building.
- (f) Mixed use buildings are to be provided with sound proof materials between the commercial and residential level.
- (g) Mixed use buildings are to be provided with one only common television antenna and/ or satellite dish, which is to be unobtrusive in appearance when viewed from the public domain.
- (h) Residential units within mixed use developments are to be provided with laundry facilities and at least one external clothes drying area, not visible from the public domain.

## E4 SPECIAL CHARACTER AREAS

Local character is what makes one area distinctive from another. It is the way an area 'looks' and 'feels'. Character is created through the interrelation of distinctive natural and built elements in the public and private domains, including topography, vegetation, streetscape, built form, activity types, as well as the emotional and cultural experience of a place. All areas in the Waverley local government area (LGA) have character, however in some, the character may be more identifiable, more unusual, or more attractive and what is important in one area might be different in another – from vibrant local centres, leafy streets, consistent dwelling typologies and architectural styles to areas and items of heritage significance, and access to coastal views and open space.

Special Character Areas are areas in the Waverley LGA which are considered to have a unique and high character value that warrant more tailored planning objectives and controls to ensure that their character is particularly protected and enhanced. Special Character Areas are selected on cultural, scenic or aesthetic grounds.

### 4.1 BONDI HEIGHTS

Bondi Heights Special Character Area applies to the area bound by Old South Head Road and Francis Street to the north, Wellington Street to the east, Bondi Road to the south and Flood Lane to the west (refer to Figure 1).



**Figure 1 Bondi Heights Special Character Area**

#### Existing Character Elements

Bondi Heights Special Character Area is located on a local topographical high point. This vantage allows district views to and from the area. It is characterised by north-south oriented streets with well-established street trees. Street blocks are generally long (700-750m) with a range of site lot sizes. A range of building types and styles exist that relate to lot sizes and development history of the area. The overall character of the area is of buildings that multiunit buildings sit in a landscape setting.

#### Desired Future Character Objectives

(a) To ensure the landscape character is the dominant image of Bondi Heights.

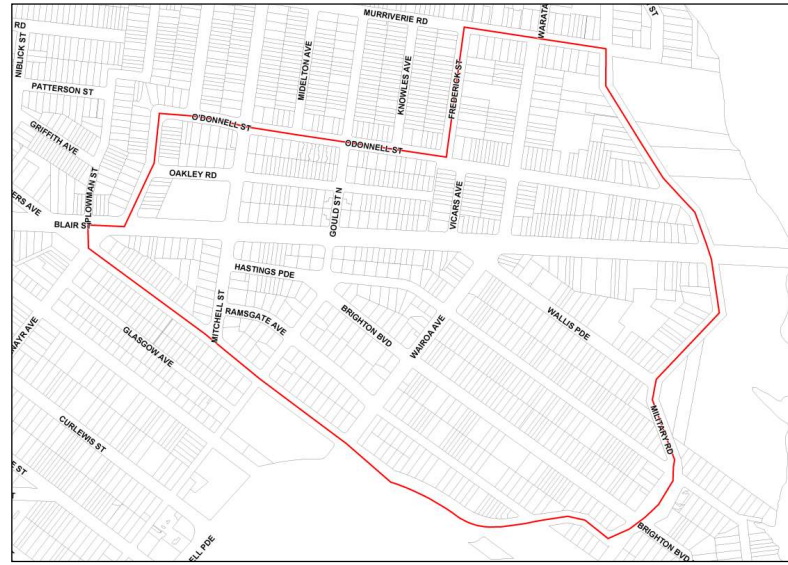
- (b) To maintain the predominant street and rear setback to provide for front gardens and planting of mature trees.
- (c) To ensure buildings respond to their location on the low and high sides of the street with respect to height and site access.
- (d) To ensure front garden walls and fences do not detract from the setting.

#### Controls

- (a) Garden walls and fences on the low side of the street are to be a maximum height of 1.2m, to allow front gardens to contribute to the streetscape. Garden retaining walls on the high side of the street are to be a maximum of 1.5m.
- (b) Front setbacks should be predominantly planted or grassed, to allow the elevated view of the front garden to contribute to the streetscape.
- (c) Outdoor terraces and decks are not permitted over garages located on the street boundary on the high side of the street.
- (d) Communal landscaped gardens are required within the front setback to contribute to the public domain.
- (e) The private open space is permitted to encroach 2.5m into the communal landscaped front setback provided that the front setback is a minimum of 6m from the street boundary.
- (f) Roof-top terraces are discouraged due to the greater potential impacts in higher density areas.

**4.2 NORTH BONDI**

North Bondi Special Character Area applies to the area bound by O'Donnell Street, Frederick Street, Murrivrie Road to the north, Military Road to the east, Campbell Parade and Warners Avenue to the south, and Glenayr Avenue to the west (refer to Figure 2).



**Figure 2 North Bondi Special Character Area**

**Existing Character Elements**

North Bondi Special Character Area has an undulating topography. The roofscape is prominent when viewed from surrounding high points. There is often a high and low side of the street. Streets generally have wide grassed verges that are sometimes privately planted (through Council's Footpath Gardens Scheme) with vegetation that contributes to the natural headland character. Regular block and lot pattern responds to the changing topographical conditions.

The predominant building stock is characterised by minimum side setbacks, consistent front setbacks and building frontages to the street whether the building type is residential flat buildings or semi-detached dwellings. Roofs are predominantly pitched and red tiled, and are visually dominant on the low side of the street. Much of the area is already developed with very little opportunity for redevelopment on infill sites.

**Desired Future Character Objectives**

- (a) To maintain the streetscape rhythm created by uniform building frontages.
- (b) To improve the amenity for residents while not detracting from the amenity of adjacent buildings.
- (c) To allow minor alterations and additions in the roof space.

**Controls**

- ~~(a) Planting should utilise minimum maintenance species growing to no more than 1m in height at maturity. The overall appearance and species selection should be compatible with the adjoining gardens. Growth must not encroach upon the footpath or obstruct pedestrian access.~~
- ~~(b) Communal landscaped gardens are required within the front setback.~~
- ~~(c) Private open space is permitted to encroach 2.5m into communal landscaped front setback provided the front setback is a minimum of 6m from the street boundary.~~
- ~~(d) The proportion of openings along street facades is to be maintained when retrofitting with balconies.~~
- ~~(e) Buildings should have pitched roofs with red tiles in keeping with the existing character of the area.~~
- ~~(f) Attics are to be secondary to the main pitched roof form.~~
- ~~(g) The established patterns of materiality and colour where there are existing rows of consistency along a street are to be maintained.~~
- ~~(h) Roof-top terraces are discouraged due to the greater potential impacts in higher density areas.~~

**4.3 BEN BUCKLER**

Ben Buckler Special Character Area is located on the northern headland at Bondi Beach and applies to the area bound by Campbell Parade and the coastline to the west, Bondi Golf Course to the north, and the coastline to the east and south (refer to Figure 3).



**Figure 3** Ben Buckler Special Character Area

**Existing Character Elements**

Ben Buckler exhibits a distinctive palisaded character of parallel streets rising to the outer southern cliff line and lined with Inter War and Mid Century residential flats and housing. Viewed from Bondi Bay, Ben Buckler presents as a dense wall of brick and painted masonry punctuated by glazed openings and a skyline of hipped tile roofs which forms a distinctive and much recognised background to the beach.

Despite the rise of topography to the north and east, streetscapes at Ben Buckler are lined with close set buildings on uniform subdivisions restricting the outlook to glimpses of Bondi Bay, the skyline to the south and the high ground of Bondi Golf course to the north. Only at the extremities of the main streets are vistas of the coastline and beach revealed.

Wide driveways and cross falls to the west, limit the amenity of otherwise wide verges landscaped with turf and sparse coastal tree species. Cranked street alignments to the northern approaches to Campbell Parade, and dense planting within properties to the low side of streets add further to the sense of enclosure.

Within this ground plan the varied styles and forms of construction are unified by orientation of balconies, decks and picture windows southwest over Bondi Bay. The visual complexity of the setting is further emphasized by a distinct separation of public and private space along all streets.

**Desired Future Character Objectives**

- (a) To maintain the headland character of Ben Buckler through the landscaping of the front gardens and appropriate planting of verges.
- (b) To maintain the rhythm of buildings frontages to the street.
- (c) To ensure side setbacks allow glimpses of the beach or ocean.
- (d) To respect the existing building character of boxy proportioned buildings, architectural elements and range of materials and finishes.
- (e) To encourage view sharing.

#### **Controls**

- (a) Planting should utilise minimum maintenance species growing to no more than 1m in height at maturity. The appearance and species selection should be compatible with the adjoining gardens. Growth must not encroach upon the footpath or obstruct pedestrian access.
- (b) Side setbacks are to be clear of obstructions to allow views between buildings to the beach.
- (c) Sites adjacent to laneways and pedestrian connections may be able to achieve increased site coverage with a reduced deep soil requirement. Where deep soil requirements are not met, this area is to be replaced with landscaped open space above ground level.
- (d) Communal landscaped gardens are required within the front setback to contribute to the public domain.
- (e) The private open space is permitted to encroach 2.5m into the communal landscaped front setback provided that the front setback is a minimum of 6m from the street boundary.
- (f) Rendered and painted finish is appropriate in this area.
- (g) Allow balconies to be provided over existing car courts for existing buildings on battle-axed blocks along Ramsgate Avenue.
- (h) Roof-top terraces are discouraged due to the greater potential impacts in higher density areas.



## E54 113 MACPHERSON STREET, BRONTE

Where there are discrepancies between this Part and other Parts of this DCP, the controls in this Part take precedence.

The following objectives and provisions apply to 113 Macpherson Street, Bronte described as Lot 19, Lot 20 and Lot 21 of DP 192094 and Lot 22 of DP 72912, also known as the Bronte RSL site (refer to Figure 57).

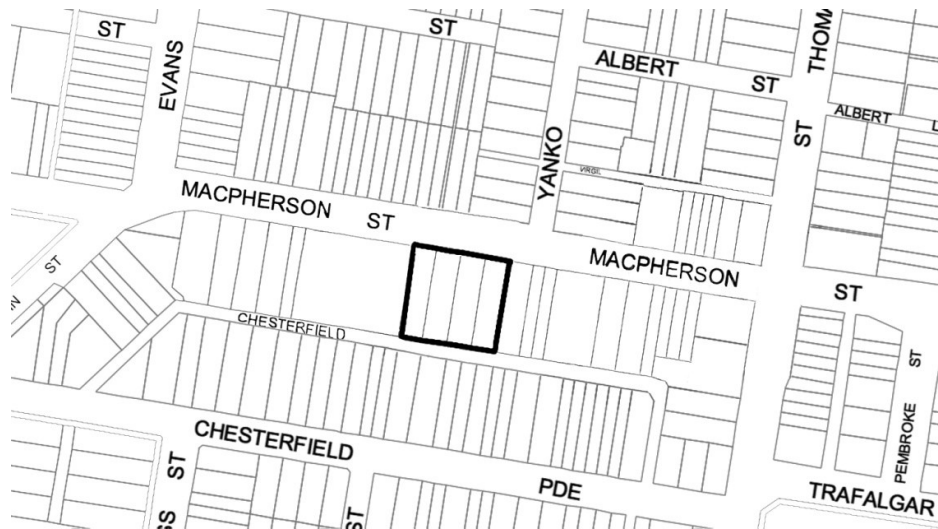


Figure 1 113 Macpherson Street Site Plan

#### 4.1 PUBLIC DOMAIN

##### Objectives

- (a) Ensure public domain benefits are provided to a high quality and in keeping with Council's vision for the neighbourhood centre.

##### Controls

- (a) Macpherson Street and Chesterfield Lane are to be landscaped to Council's requirements.
- (b) Street furniture and renewal of paving is to be provided to Macpherson Street and Chesterfield Lane to Council's requirements.

## 4.2 BUILT FORM

### Objectives

- (a) Facilitate the redevelopment of the site to achieve a high quality urban form.
- (b) To ensure that redevelopment does not result in adverse impacts on the amenity, privacy and solar access of existing and future residential premises within the precinct.
- (c) To facilitate built form that accounts for the change in level between Macpherson Street and Chesterfield Lane.
- (d) To set building heights and frontage alignments to respect the existing character and desired future character of the Bronte's Macpherson Street and St. Thomas Street Neighbourhood Centre.
- (e) Ensure that development has high architectural quality and diversity, and strongly defined streets.
- (f) To ensure that new development reflects the historical subdivision pattern and established rhythm of the main street retail buildings located east of the site.

### Controls

- (a) The development of 113 Macpherson Street is to be in accordance with the development control envelope illustrated in Figures 58 and 59.
- (b) Development is to comply with *Part B16 Public Domain*.
- (c) Provide awnings to the entire Macpherson Street frontage between the ground and first floor, except over the driveway. Refer to *Part B16.4 Awnings and Colonnades*.
- (d) Buildings are to be built to the street and lane alignments.
- (e) No less than 90% of the building is to be aligned to the street boundary for the ground and first floor fronting Macpherson Street.
- (f) Provide setbacks above the street-wall in accordance with Figures 58 and 59.
- (g) Provide side setbacks in accordance with Figures 58 and 59.
- (h) Each retail unit must present to Macpherson Street with a frontage no greater than 6m wide.
- (i) The Macpherson Street facade must be articulated to reflect the established 6m / 12m rhythm of the existing main street retail buildings located east of the site.



### Legend



PROPOSED BUILDING ENVELOPE



EXISTING BUILDINGS

4

HEIGHT OF BUILDINGS (IN STOREYS)

6

DIMENSIONS (IN METRES)

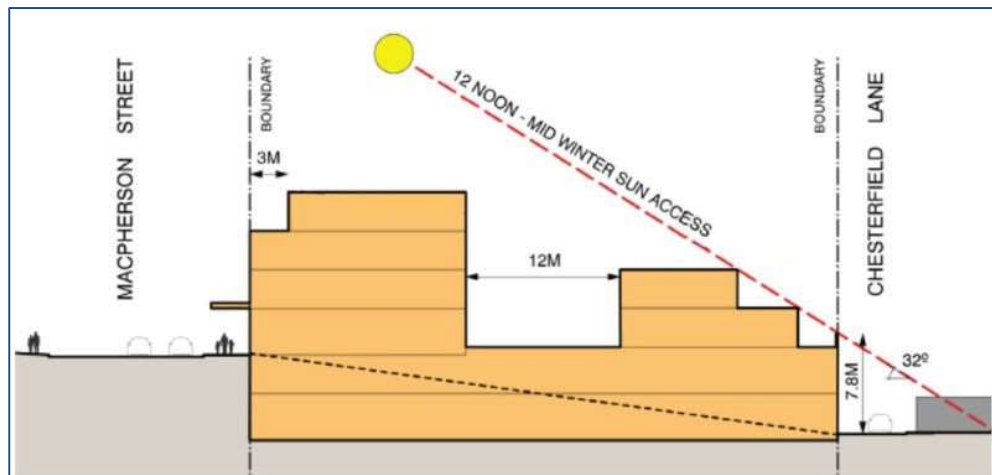


NON RESIDENTIAL VEHICULAR AND  
LOADING ACCESS



RESIDENTIAL VEHICULAR ACCESS

**Figure 58** Development Control Envelope



**Figure 1** Development Control Envelope section

### 4.3 ACTIVE STREET FRONTAGES

#### Objectives

- (a) To promote pedestrian activity and safety in the public domain.
- (b) To provide a high degree of surveillance over Macpherson Street and Chesterfield Lane
- (c) To provide transparency and visual contact between the public domain and the building interior.
- (d) To ensure that retail premises present a “public face” to enhance the character and vitality of the neighbourhood centre.

#### Controls

- (a) Active street frontages are required at footpath level along Macpherson Street.
- (b) Not more than 10% of the Macpherson Street frontage can be blank walls or service areas.
- (c) Uses providing passive surveillance of Chesterfield Lane must be provided for the majority of the width of the ground and first storey fronting Chesterfield Lane. Car parking must be sleeved by a commercial or residential use.

## 4.4 TRANSPORT

### 4.4.1 Loading Facilities

#### Objectives

- (a) To ensure that non-residential uses do not result in adverse impacts on the amenity of existing and future residential premises, schools, childcare centres and community facilities.

#### Controls

- (a) Driveway entry and exit to commercial loading docks is restricted to Macpherson Street.
- (b) The driveway access to loading facilities and parking must be combined.
- (c) Loading facilities must be located internally on the site. They must not front Macpherson Street.

### 4.4.2 Driveways and Car Parking Access

#### Objectives

- (a) To ensure that non-residential uses do not result in adverse impacts on the amenity of existing and future residential premises, schools, childcare centres and community facilities.
- (b) To ensure main streets are not dominated by driveways.
- (c) To encourage continuous main streets.
- (d) To ensure safety for pedestrians on heavily used footpaths.

#### Controls

- (a) The width of the driveway on Macpherson Street must be no greater than 9m wide.
- (b) The driveway off Macpherson Street must be located at the western end of the front boundary as shown on the development control envelope (refer to Figure 58).
- (c) Access to residential parking is permitted from Chesterfield Lane.
- (d) Access to commercial, retail and RSL club parking is not permitted from Chesterfield Lane.
- (e) Provide a maximum gradient of 1 in 20 (5%) for the car park access driveway for the first six metres within the site.
- (f) The driveway access must be fully enclosed where located more than six metres from the Macpherson Street site boundary in order to provide acoustic attenuation for the residential apartments to the west of the site.

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#### **4.4.3 Non – Residential Parking Rates**

##### **Objectives**

- (a) To provide dedicated car parking for those working at the development.

##### **Controls**

- (a) Of the total number of non – residential parking spaces provided, 80% is to be allocated for visitors / short-stay parking, and 20% is to be allocated for employee / long-stay parking.

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#### **4.4.4 Bicycle Parking**

##### **Objectives**

- (a) To provide accessible secure and safe bicycle parking close to major pedestrian entries.

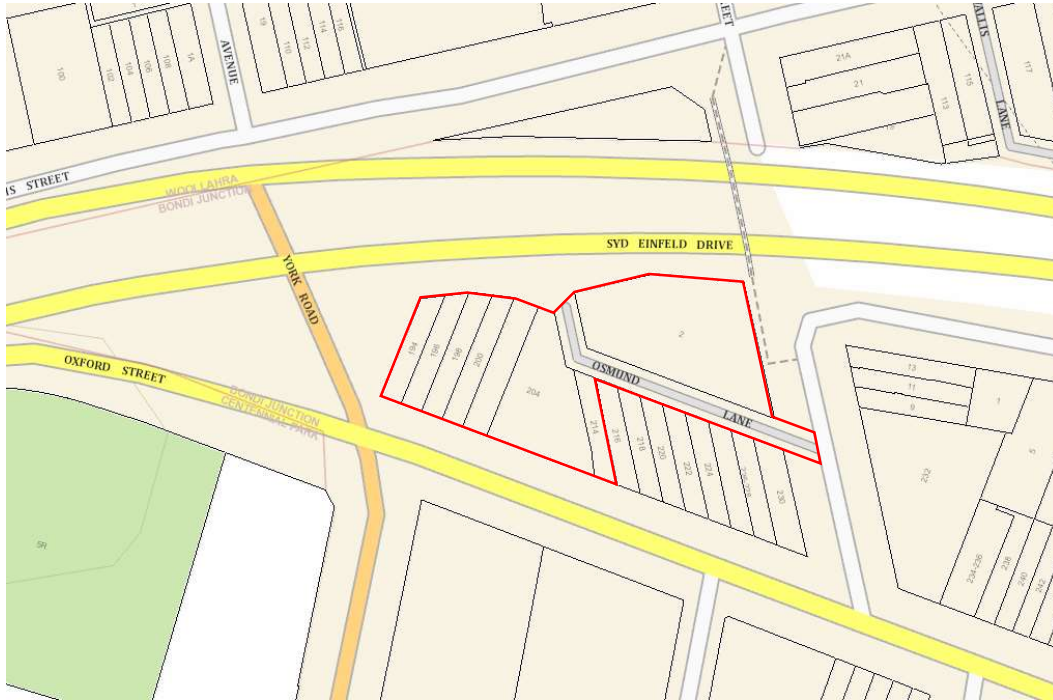
##### **Controls**

- (a) Provide minimum 50% of the required bicycle parking for non-residential premises at an accessible on grade location near the main pedestrian Macpherson Street entries.



**E65 194-214 OXFORD STREET, 2 NELSON STREET AND OSMUND LANE, BONDI JUNCTION**

The following objectives and controls apply to 194-214 Oxford Street, 2 Nelson Street and Osmund Lane, Bondi Junction described as Lots 10, 11, 12 and 13 DP 260116, Lot 16 DP 68010, Lot 1 DP 79947, Lot 1 DP 708295 and SP 34942 (refer to Figure 60).



**Figure 1** Subject sites outlined in red

Where there are discrepancies between the controls of the DCP and others within this site specific DCP, these controls take precedence.

The intention of this site-specific DCP is to encourage and facilitate innovative design that reflects the landmark significance of this site. The controls in this Part of the DCP are not intended to be prescriptive if it can be demonstrated that the design has achieved the objectives of this Part through minor discrepancies from this Part. This Part of the DCP has been created with regard to the following Planning Principles for the site:

- An acceptable human scale at the podium height
- Slender towers to mitigate visual amenity impacts
- Good public amenity on the site
- Significant landscaping on site including deep soil planting and substantial mature tree planting

## 5.1 BUILT FORM

### Objectives

- (a) To deliver the highest standard of architectural, urban and landscape design.
- (b) To establish building envelopes that minimise overshadowing on Centennial Park and surrounding low scale residential areas.
- (c) To ensure buildings are environmentally innovative particularly with regard to water and energy conservation.
- (d) To ensure development does not adversely impact on the significance of the neighbouring heritage buildings, landscape and conservation areas.

### Controls

- (a) A two/three storey podium is to be provided fronting Oxford Street.
- (b) Architectural form is to be articulated to address the corner of Oxford Street, York Road and Syd Einfeld Drive.
- (c) The articulation of the podium level is to reflect a fine grain, terrace-like subdivision pattern consistent with the existing terrace subdivision pattern along West Oxford Street.
- (d) The towers are to be set back six metres from the edge of the podium fronting Oxford Street. A lesser setback may be considered if it can be demonstrated that it achieves the objectives of this DCP.
- (e) The tower location must minimise overshadowing of Centennial Park and the surrounding low scale residential areas.
- (f) No additional height or floorspace ratio above the LEP controls will be considered for these sites.
- (g) Floor space in podium levels must be used for commercial and retail uses. Any storage space, servicing areas, car parking or other areas that are not defined as calculable floor space under Waverley LEP must be located below ground.

## 5.2 DESIGN EXCELLENCE

### Objectives

- (a) To deliver the highest standard of architectural, urban and landscape design.
- (b) To maximise the overall environmental performance of new buildings.

### Controls

- (a) The tower location must minimise overshadowing of Centennial Park and the surrounding low scale residential areas.
- (b) The development must minimise any impacts on heritage items in the vicinity of the site.
- (c) The development must minimise any visual impact of the buildings when viewed from Centennial Park and the Public Domain.
- (d) The development must incorporate Ecologically Sustainable Development Principles to minimise carbon emissions, potable water use, energy use and waste.
- (e) The internal planning and façade design of the buildings are to address and ameliorate the significant road noise.
- (f) All materials must be naturally finished, low maintenance, contextually appropriate and painted surfaces are discouraged. Materials used shall be durable and weather well over time.
- (g) Air conditioners are not to be located on balconies.

**5.3 PUBLIC DOMAIN****Objectives**

- (a) To provide a high quality and safe public domain with high pedestrian amenity that benefits the wider community.
- (b) To facilitate pedestrian movement and priority throughout the entire site.
- (c) To ensure the highest standard of architectural, urban and landscape design.
- (d) To provide public art in prominent and publicly accessible locations.
- (e) To facilitate substantial planting to mitigate any visual impacts the towers may have from Centennial Park and surrounding areas.

**Controls**

- (a) As agreed in the Voluntary Planning Agreement, a public plaza totaling a minimum of 311m<sup>2</sup>, a pedestrian/cycleway through site link from Oxford Street to Osmund Lane totaling 136m<sup>2</sup> and approximately 208m<sup>2</sup> of footpath widening is to be provided.
- (b) The public plaza must receive 3 hours solar access to a minimum of 50% of its area on 21 June.
- (c) The area within the drip line of the heritage listed Norfolk Island pine tree must incorporate permeable materials.
- (d) A through-site link is to be provided between Oxford Street and Osmund Lane uncovered by any structure (except for the building awning) and must be publicly accessible between the hours of 7am-10pm Monday to Sunday.
- (e) Active frontages are to be provided to all public places at street level.
- (f) Street furniture and public art is to be provided within any public plaza in accordance with Council's Public Domain Improvement Plan and Public Domain Technical Manual.
- (g) Awnings and footpaths are to be provided on all active frontages.
- (h) The 3.5m of land dedicated along Oxford Street will be used for mature street tree plantings and landscaping purposes.
- (i) Stormwater drainage locations and landscaping are to be detailed in any plans submitted as part of a design excellence competition.
- (j) Under awning lighting is to be provided to achieve appropriate luminance levels for pedestrians (refer to relevant Australian Standards). Lighting should be recessed into the soffit of the awning.
- (k) Landscaping and design of the public domain is to be high quality and incorporate features such as indigenous tree species and landmark sculptural elements.
- (l) A Landscape Plan for the proposed public plaza and public realm surrounding the development is required to be submitted in accordance with the *Waverley Development Application Guide* and include:
  - i. Substantial planting of mature trees fronting Oxford Street, Syd Einfeld Drive and York Road; and
  - ii. A schedule of the common name and scientific name of species to be planted, the size and number; and
  - iii. A plan showing the location of the plants in the schedule and all deep soil planting.
- (m) An independent arborist report must be submitted to Council prior to the commencement of any design excellence competition. The report will detail all

existing trees on and adjacent to the site and outline all trees that can be retained based on AS 4970 – Protection of trees on development sites.

- (n) A communal rooftop space landscaped with drought tolerant Australian native plants must be provided for resident access and use on both towers.
- (o) Podiums are to be landscaped with drought tolerant Australian native plants.
- (p) A Public Art Plan is to be prepared and submitted that outlines public art for the proposed public plaza and public realm surrounding the development.
- (q) The ongoing management of any public domain or plazetta provided is to be the responsibility of the body corporate.
- (r) Any land dedicated for public use must only be used for public use and cannot be used for vehicle access, garbage truck access or space required for the operation or maintenance of the towers.

## 5.4 WASTE

### Objectives

- (a) To ensure new developments and changes to existing developments are designed to minimise waste generation and maximise resource recovery.
- (b) To encourage waste storage facilities that are designed to enable source separation for recovery.
- (c) To ensure waste and recycling systems are easy to use and complement Council's waste and recycling services.
- (d) To promote safe practices for storage, handling and collection of waste and recycling.
- (e) To prevent stormwater pollution that may result from poor waste and recycling storage and management practices.
- (f) To minimise amenity impacts during the storage, use and collection of waste and recyclables.
- (g) To prevent impacts to the environment that may result from litter, excess waste and illegal dumping.
- (h) To minimise interference of waste collection on pedestrian access, safety and amenity.
- (i) To minimise interference of waste collection on local traffic.

### Controls

#### 5.4.1 General

- (a) The Site Waste & Recycling Management Plan (SWRMP) is to be submitted in accordance with the Waverley Development Application Guide.
- (b) Waste storage space is to be designed with flexibility to accommodate a future change in use to a use with a higher waste generation rate.

#### 5.4.2 Amenity

- (a) Waste and recycling storage areas must be visually and physically integrated into the design of the space.
- (b) Waste and recycling storage areas must be designed and located to avoid adverse impacts on the amenity of adjoining sites including noise, odour and visual impacts.
- (c) All public place waste and recycling receptacles must align with council's Public Domain Technical Manual.

#### 5.4.3 Access and Collection

- (a) On-site waste collection is to be accommodated within a basement or at grade within the buildings from a dedicated collection point or loading bay that does not impede pedestrian or vehicle movement within the development.
- (b) The on-site waste collection point is to be of a sufficient size to store all bins to be collected without interruption to the functioning of the development.

#### 5.4.4 Ongoing management

- (a) Ongoing management of the area is to be in accordance with the approved site waste and recycling management plan (SWRMP) of each development in the area to ensure that appropriate waste and recycling services are provided.
- (b) The SWRMP must be re-evaluated every 5 years and amended with relevant information.
- (c) Waste generated by a development must not exceed the maximum permitted generation rates for each building's use.



## 5.5 ACCESS AND PARKING

### Objectives

- a) To minimise the impacts of vehicles on pedestrian amenity and public spaces.
- b) To encourage a safe and practical space for all transport modes.
- c) To minimise any additional pressure on the existing parking in West Bondi Junction.

### Controls

- a) Osmond Lane is to be retained as a 7.0 metre local access lane with “No Parking” restrictions on both sides.
- b) Access to and from Osmond Lane at Nelson Street is to be left-in left-out with enforcement by signs or a central raised median on Newland Street.
- c) The pedestrian bridge over Syd Einfeld Drive and ramps/stairs is to be retained. The bridge should be easily accessible from the site and the pedestrian and cycleway through site links.
- d) Resident parking should be provided on site
- e) The 3.5m road widening dedication along Oxford Street is not required for traffic related uses and as such can be allocated for other purposes.
- f) Resident parking for this development will not be included within area 22 of the Waverley Resident Planning Parking Scheme (RPPS).

5.6 PEDESTRIAN AND CYCLEWAY CONNECTIONS



Figure 2 Cycleway connection route\*

\*Note: The Waverley 2013 Bike Plan designates the cycleway on this site for mixed traffic.

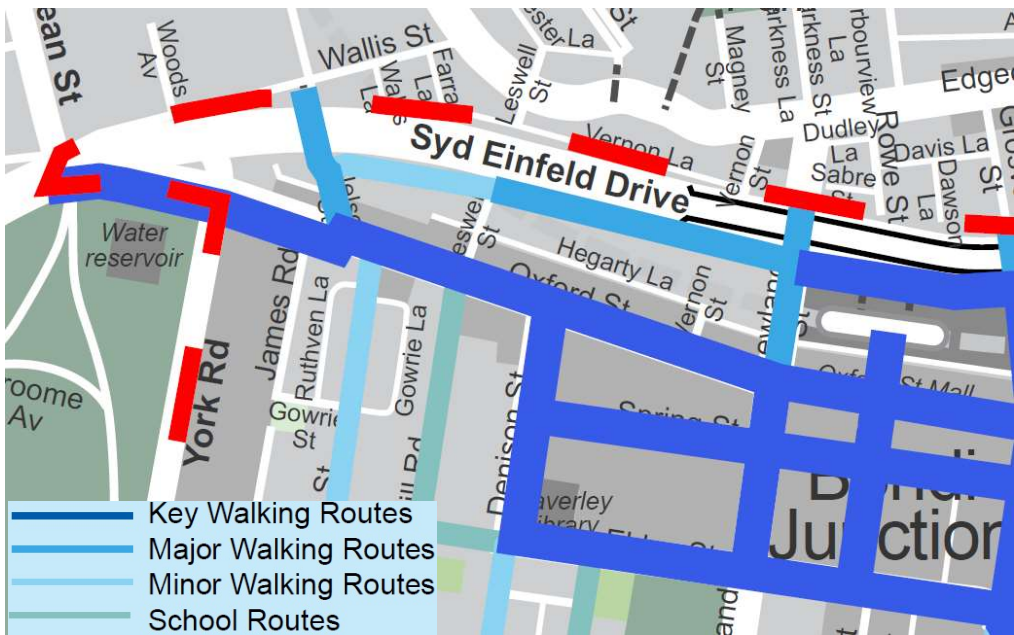


Figure 3 Pedestrian connection route

## ANNEXURES

### ANNEXURE E1-1 WIND TUNNEL STUDY

Wind Tunnel Study is to be prepared for all building over 9 storeys in height or is considered exposed. This is the most definitive method of modelling wind effects. Wind effects are modelled in a wind tunnel facility and local speeds are measured at the various critical outdoor areas within and around the site and compared directly against the relevant comfort criteria. Any recommendations for treatments such as altering the building form, the implementation of awnings, canopies, strategically placed screens or dense planting to protect entrances or podium areas should be modelled in the wind tunnel and tested.

#### Requirements for the preparation of a wind tunnel report

1. The Wind Tunnel Study required under this plan should
  - (a) Assess the likely wind effects of the development;
  - (b) If the wind conditions in any of the areas surrounding the site exceed the relevant criteria then model the existing wind conditions to accurately quantify the impact; and
  - (c) Recommend measures required to improve adverse wind conditions created by the proposal and demonstrate that the recommended measures will be effective in mitigating the adverse wind effects.
2. Wind tunnel tests must be carried out as follows:
  - (a) Surround models are to be placed around the model of the proposed building to a radius of approximately 500m. The model scale should not be smaller than 1:500.
  - (b) The boundary layer flow pertaining to the upstream terrain from the various wind angles must be reproduced at the appropriate scale. This includes the modelling of the variation with height, of mean velocity and turbulence intensity of the wind, up to the height of the boundary layer. Other modelling parameters that must be considered are the integral scale of turbulence of the wind, the effect of scale on the Jensen and Reynolds numbers and a zero longitudinal pressure gradient. The Jensen and Reynolds numbers are dimensionless numbers used to predict full scale results from tests performed using reduced scale models. Note that the mean wind speed and turbulent intensity boundary layer profiles must be modelled to within 10% of the target values. It is recommended that the target values be based on the Deaves and Harris (1978). Bondi Junction Centre Waverley Development Control Plan 2010. The integral scale of turbulence must be matched to within a factor of 3. The maximum permissible blockage is 10% (maximum sectional area of the model divided by the sectional area of the wind tunnel test section). The maximum height of the model must not exceed half the height of the wind tunnel test section. The minimum permissible Reynolds Number is  $5 \times 10^4$ .
  - (c) Measurements of local wind velocities should be based on the maximum 2 to 3 second duration gusts (in full scale), taken from a sample length of 1 hour (in full scale). If the gust-equivalent mean criteria are used then the mean and local turbulence intensity should also be measured.

- (d) Analysis of the wind effects must be based on measurements taken from an adequate number of locations, covering all the potentially affected areas. For each of the locations, wind speed measurements should be taken from a minimum of 16 wind directions. Initial tests to be performed without the effect of the proposed trees or other wind mitigation devices.
- (e) Analysis of results must be based on reliable meteorological data for Sydney (preferably from the Sydney Airport Observation office), taken over a minimum of 30 years of continuous data. In the case where treatments are required, their effectiveness must be confirmed with further wind tunnel measurements.

## ANNEXURE E2-1 DESIGN GUIDELINES

The elements shown in each group are drawn from buildings in the Bondi Beachfront Area and represent a selection of representative types and building scale for reference purposes. In all cases, site specific requirements and physical parameters will affect the design solution. As well, the vitality of individual choice extends and enriches the design process.

The guidelines are provided as diagrams rather than being prescriptive. This allows interpretation with the wide range of materials and styles while at the same time, providing variety and flexibility, thereby uniting the street in urban design terms and providing a high degree of continuity.

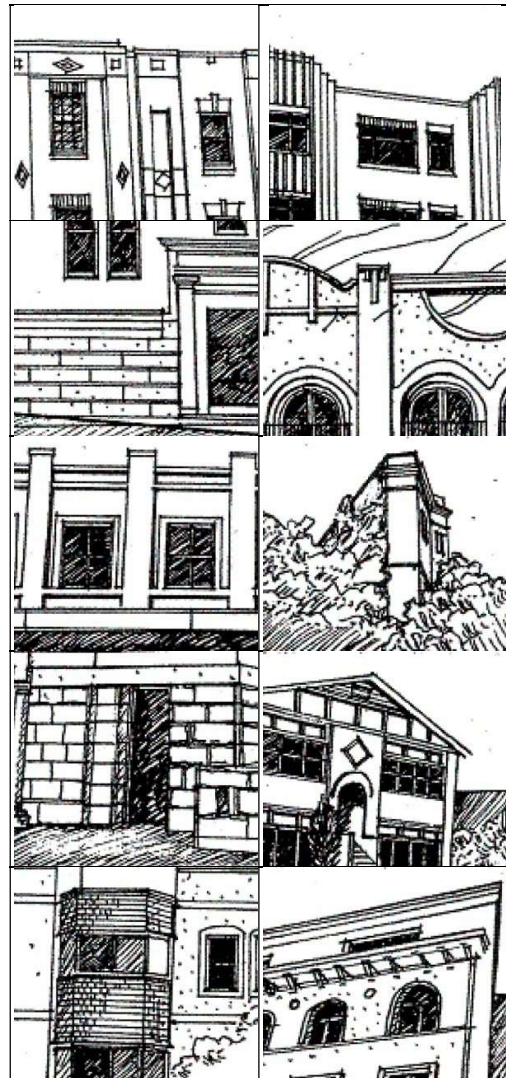
### WALLS

The Bondi Beachfront Area includes buildings with almost every type of masonry wall finish, with timber used as panelling in gables, balconies, bay windows and other secondary uses.

The embellishment of walls, roofs and parapets exemplifies the stylistic differences of each succeeding period. Walls and their concluding parapets are visually important and the development of wall surfaces with a multiplicity of textures and patterns also provides interest and character to otherwise bleak buildings. New buildings should continue this tradition and avoid the bland unornamented brick surfaces of recent unit development.

Materials include:

- Brick
- Render
- Stone
- Roughcast
- Fibre Cement Sheet
- Shingles
- Timber

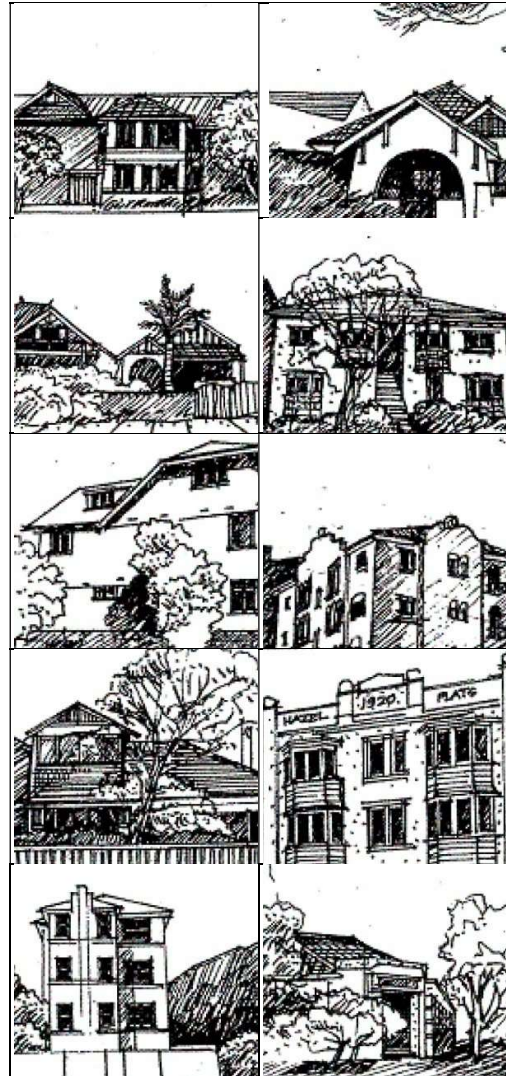




**ROOFS**

No one type of roof type predominates with most forms of roof represented in the Bondi Beachfront Area. The resultant architectural variety provides constant visual interest and is to be encouraged. Junctions of roof and wall also vary, with a wide range of eaves and parapet types used singly and often in combination.

Continuation of this character is to be encouraged and flat roofs without parapets are generally to be avoided

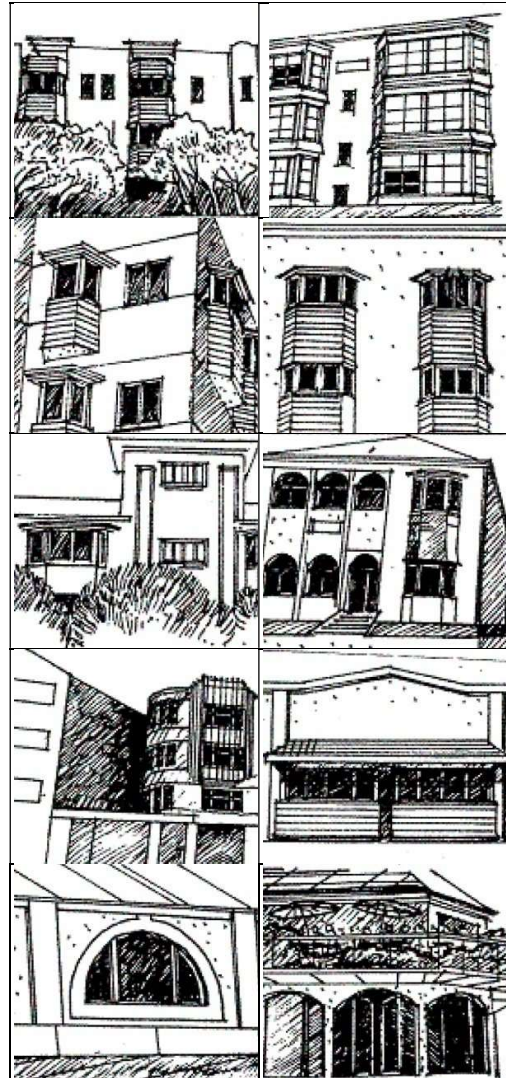


## WINDOWS

Buildings in the Bondi Beachfront Area have a rich variety of window types, which reflects the resort character of the area.

Much building in the area is at an urban scale with use of details appropriate to the larger scale. Externally, the architecture of the area and the corresponding window type varies from Victorian, through various Interwar styles including Art Deco and Spanish Mission to the faceless expression of four storey walk-ups and developer modern.

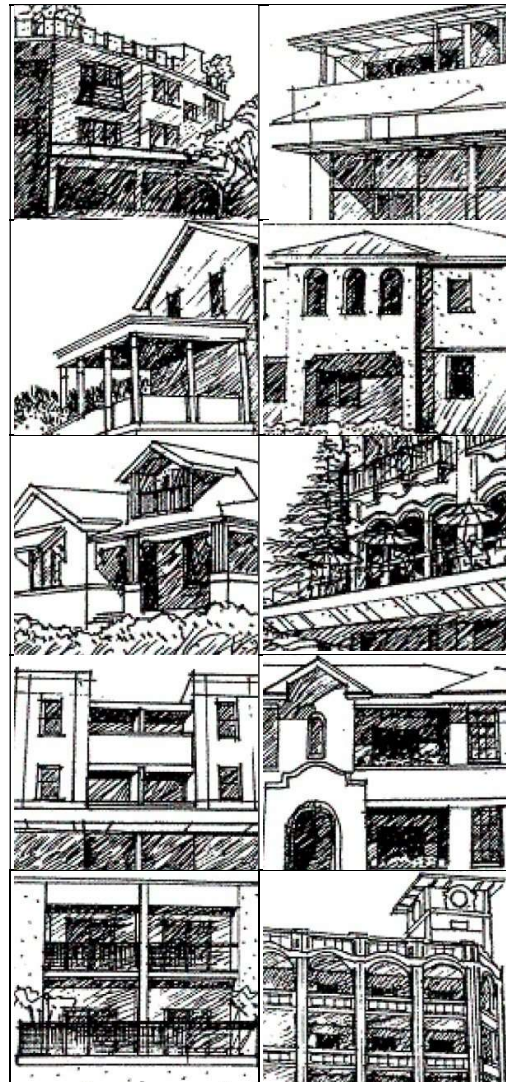
Windows reveal extraordinary inventiveness and variation of size, shape and detail. In addition to the variety of types and styles of standard windows, there are numerous types of bay windows, which provide greater access to views and sunlight. The continued use of windows that enrich and enliven the facades of buildings in the core area is desirable. It should be noted that the range of windows illustrated is by no means exhaustive.





**BALCONIES AND VERANDAHS**

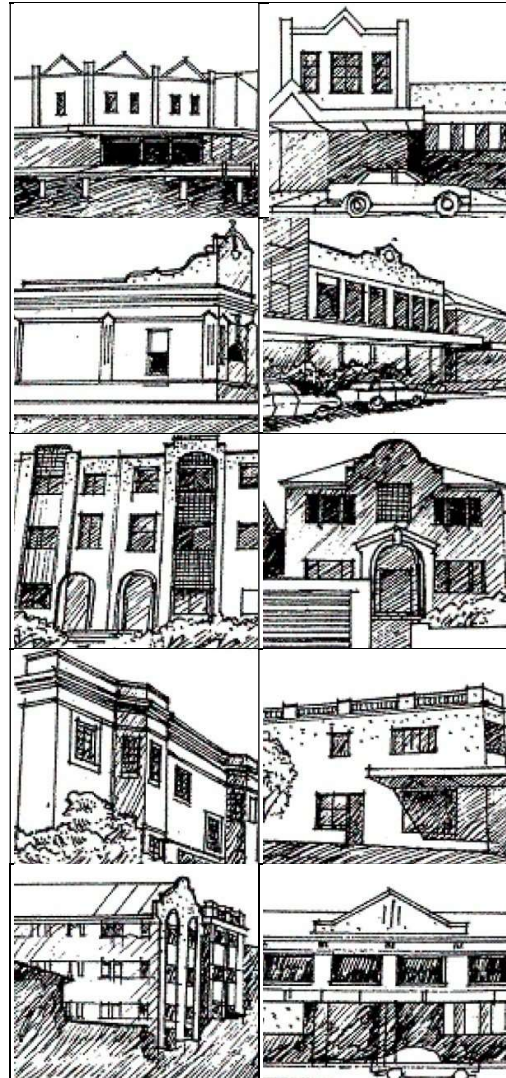
Balconies and verandahs are evident throughout the Bondi Beachfront Area in all types and scales. Widely used in the area are large covered balconies or verandahs in flat buildings. These spaces function effectively as outdoor rooms for recreational use in summer, giving views, light and air to flats which would be otherwise small and without immediate access to external open space. In addition to the “traditional” verandah room evident on much flat development, terraces in first floor awning locations and at top floor levels set back behind balustrades are to be encouraged.



**PARAPETS**

Parapets form a distinct and characteristic element in commercial and larger scale residential buildings. Styles in the Bondi Beachfront Area include Classical, Victorian, Art Deco, Spanish Mission and other hybrid types. The various styles are used to effect on both linear and corner elevations, enlivening buildings of utilitarian character that would otherwise be commonplace.

The use of all types of parapets in new development is to be encouraged to continue to develop the building traditions in the Bondi Beachfront Area.



**COLOUR**

Colour in the Bondi Beachfront Area reflects both periodic change in community taste and the availability of building materials over time. In terms of natural materials, the use of sandstone as a plinth with the characteristic brown dry pressed or the clinker burnt purple brick above provides a traditional colour palette.

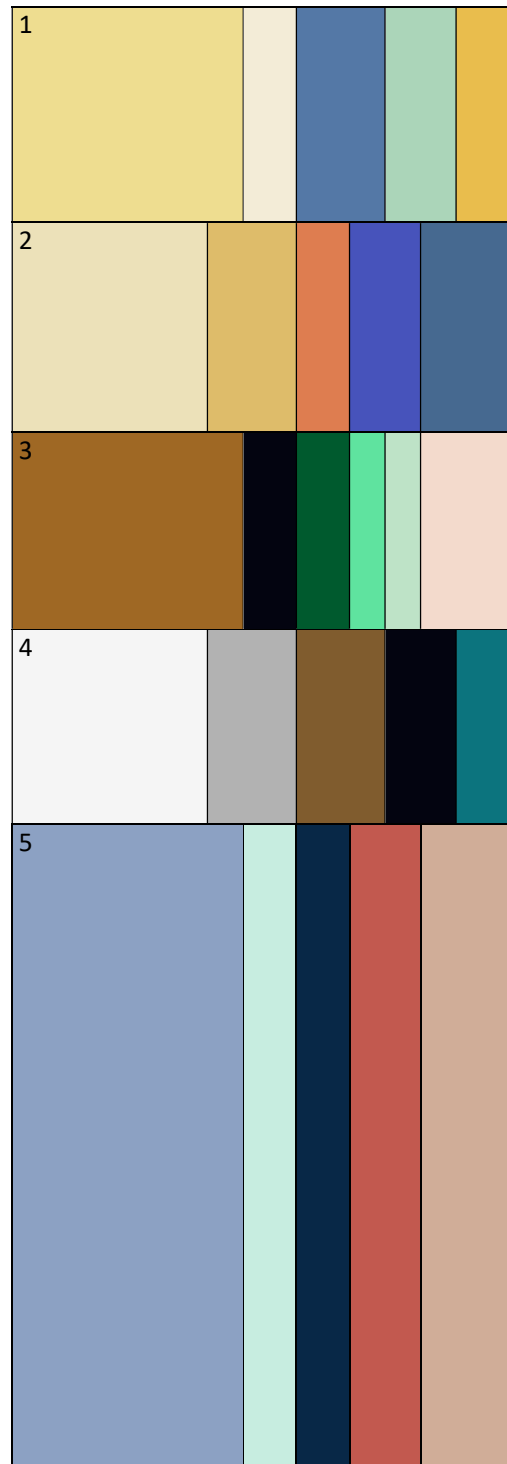
Colours shown at the top right (1) are typical of the colour palette for Campbell Parade adopted by Council in 1988. This scheme reflected in the beachside character of the area with complementary sand and sea colours in the high to mid tone range.

Four alternative schemes are shown (2 - 5). These stay within the sand and sea: range but strengthen tonal contrast and include richer, more saturated hues. Whilst embracing a broader range of colours, the proposals avoid the purple/red segment of the spectrum, and mud/olive colourings.

Sample colours taken from the Taubmans range are (left to right):

1 Sambu (T22-3W) Cameo Lace (T2-3W) Mariner Blue (T74-7A) Portolina (T79-4W) Golden Globe (T22-6A)	4 Portland Stone (T122-2W) Woollooware (T122-5W) Mascari (T116-7A) Mojo (T172-8B1) Deep Splendor (T79-7A)
2 Seersucker (T106-3W) Warm Ochre (T22-5A) Russet Ridge (T130-7A) Saxon Blue (T62-8N) Blue Masque (T71-8A)	5 Dragonfly (T151-5W) Shy Green (T152-3W) Sea Deep (T149-8N) Earth Tone (T34-7A) Baked Dough (T27-4W)
3 Plaza Buff (T115-80) Mojo (T172-8B1) Emerald Turp (T82-8N) Fantasy Green (T81-7A) Oceanic Forest (T83-2W) Lambs Tail (T116-1W)	

*Please note the sample colours shown are indicative only.*



The colour ranges are provided for guidance in the development of colour schemes appropriate for each building, with reference to size, location, style and other specific conditions. They are therefore not definitive colour schemes but should be regarded as an indication of the scope of colour suitable for the preparation of schemes for individual properties.

Some general principles apply:

- (a) Upper storeys which are set back should be the same colour or preferably a darker colour, as the lower floors of the building as light or strong colours visually come forward.
- (b) Strong elements of the façade should be visually balanced, e.g. in general terms, vertical elements such as columns and pilasters look best linked with horizontal elements painted the same colour.
- (c) Under awning and colonnades, high tones should be used to reflect both artificial and natural light.
- (d) When economy dictates a limited palette, select a lighter tone to emphasise the modelling of desirable architectural detail. Darker tones will reduce the visibility of poorly detailed facades.

### ANNEXURE E3-1 TYPICAL BUILT FORM FOR TWO STOREY CENTRES

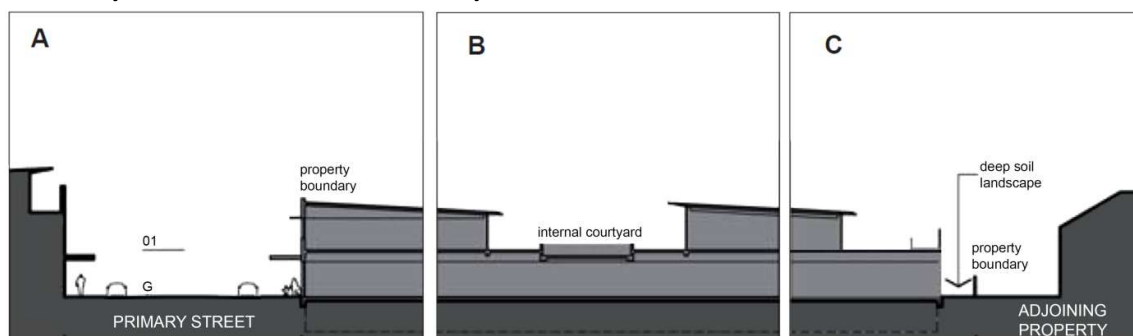
Annexure E3-1 applies to the following centres:

1. Murrivier Road ~~Neighbourhood~~ Centre.
2. Murray Street ~~Neighbourhood~~ Centre.
3. Bronte Street (Macpherson) ~~Neighbourhood~~ Centre

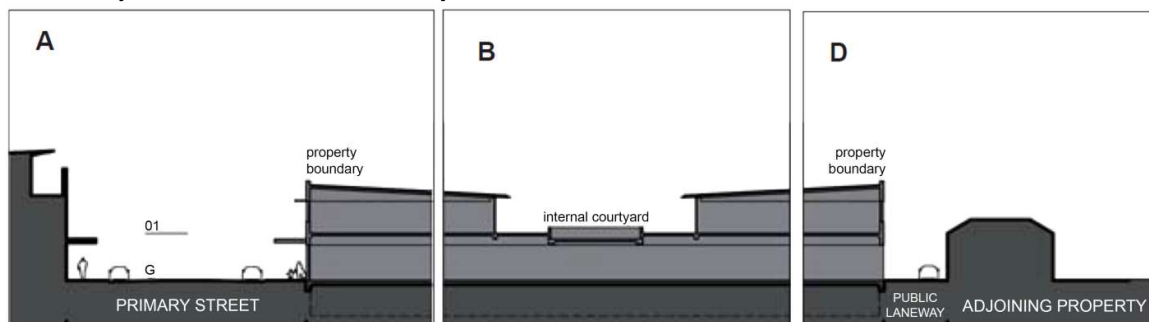
There are two typical built forms for two storey Local Village Centres which are dependent on whether the property has access to a rear lane.

- a) Properties without rear laneway: Control Diagram A, B and C.
- b) Properties with rear laneway access: Control Diagrams A, B and D.

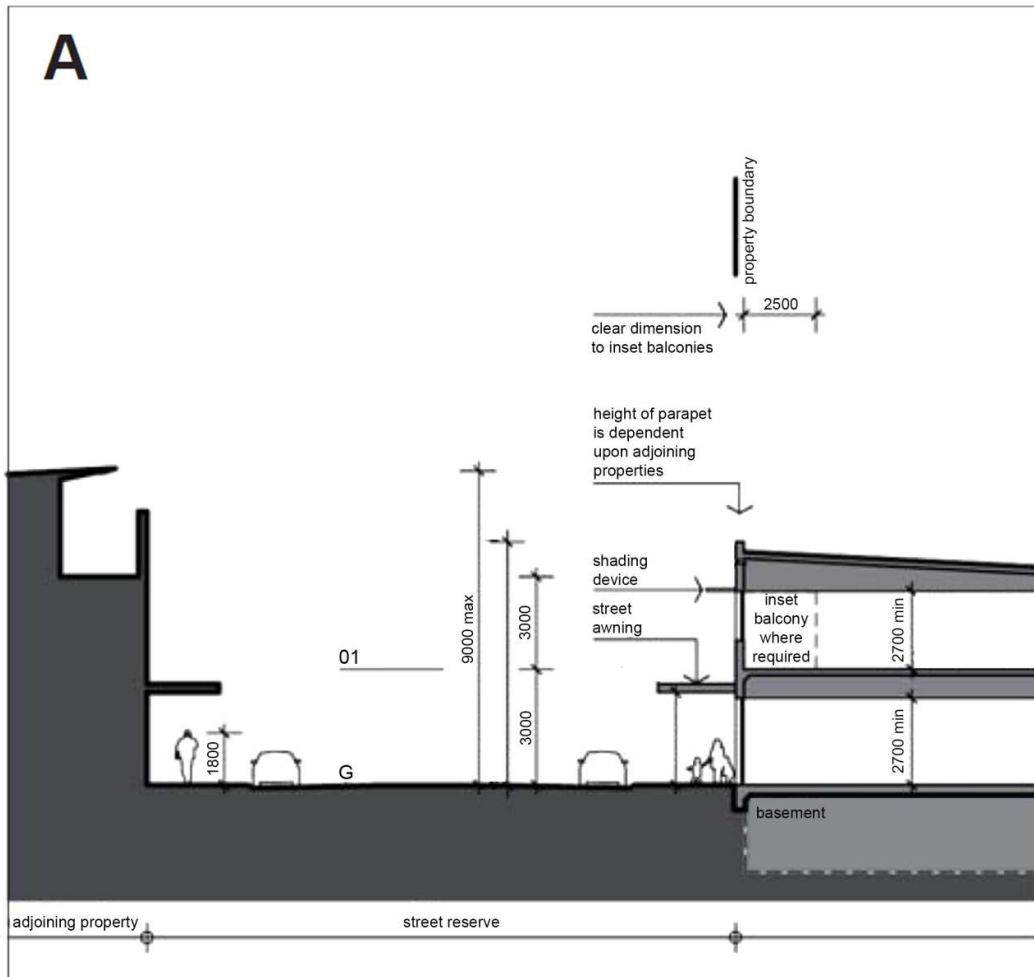
#### Two storey section – without rear laneway



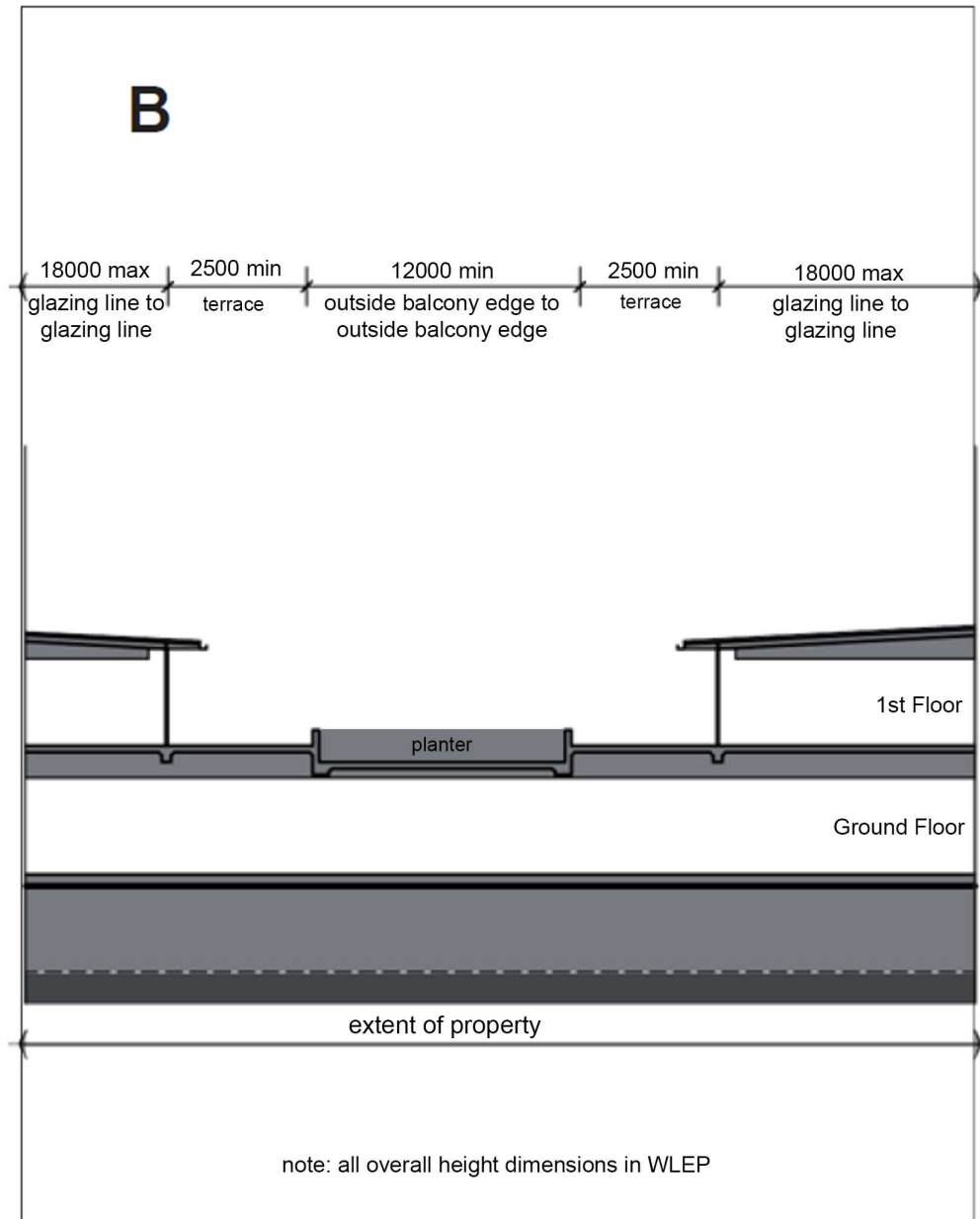
#### Two storey section – with rear laneway



Two (2) storey detailed street frontage and internal floor to ceiling heights

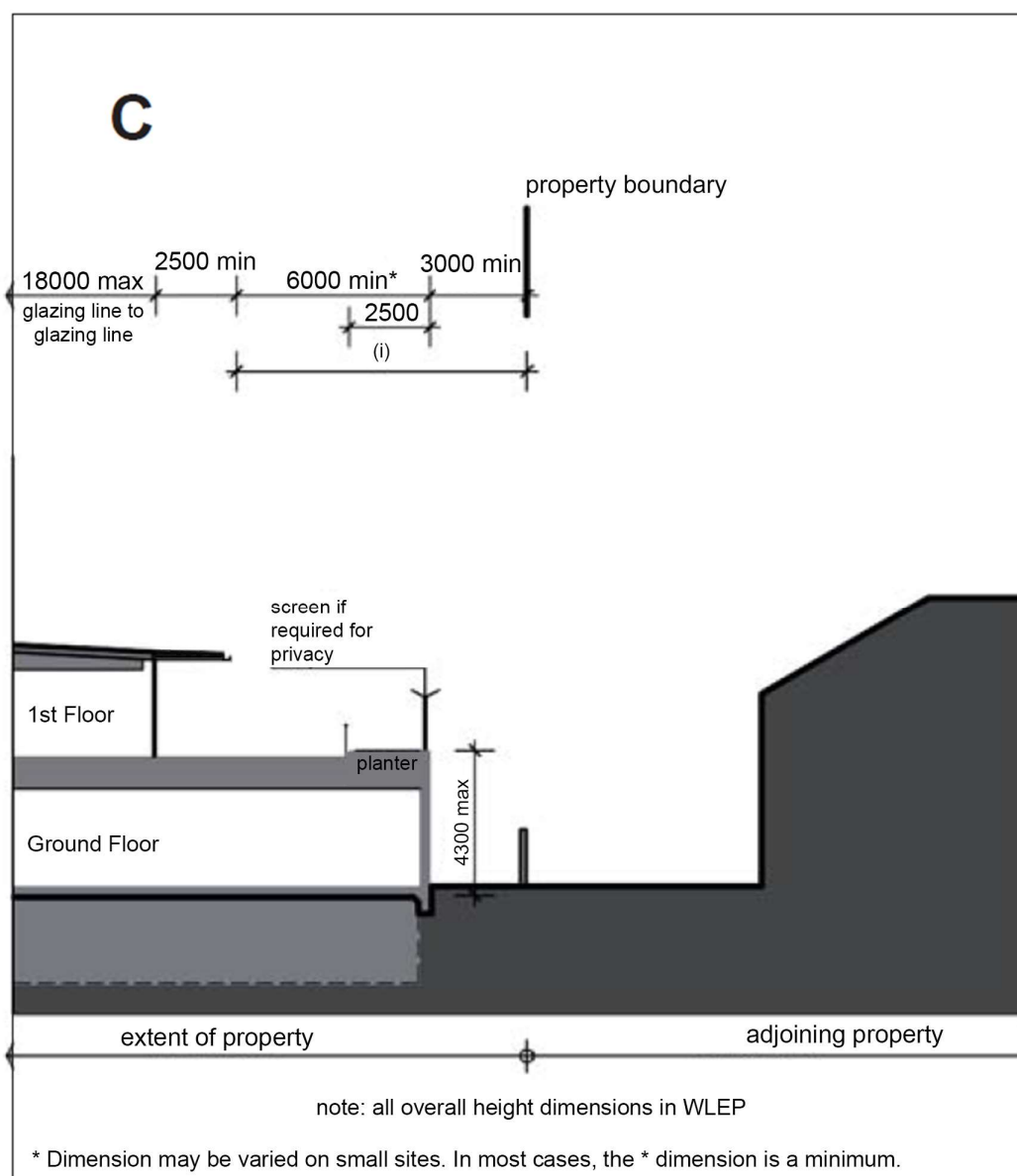


Two (2) storey detailed internal courtyard

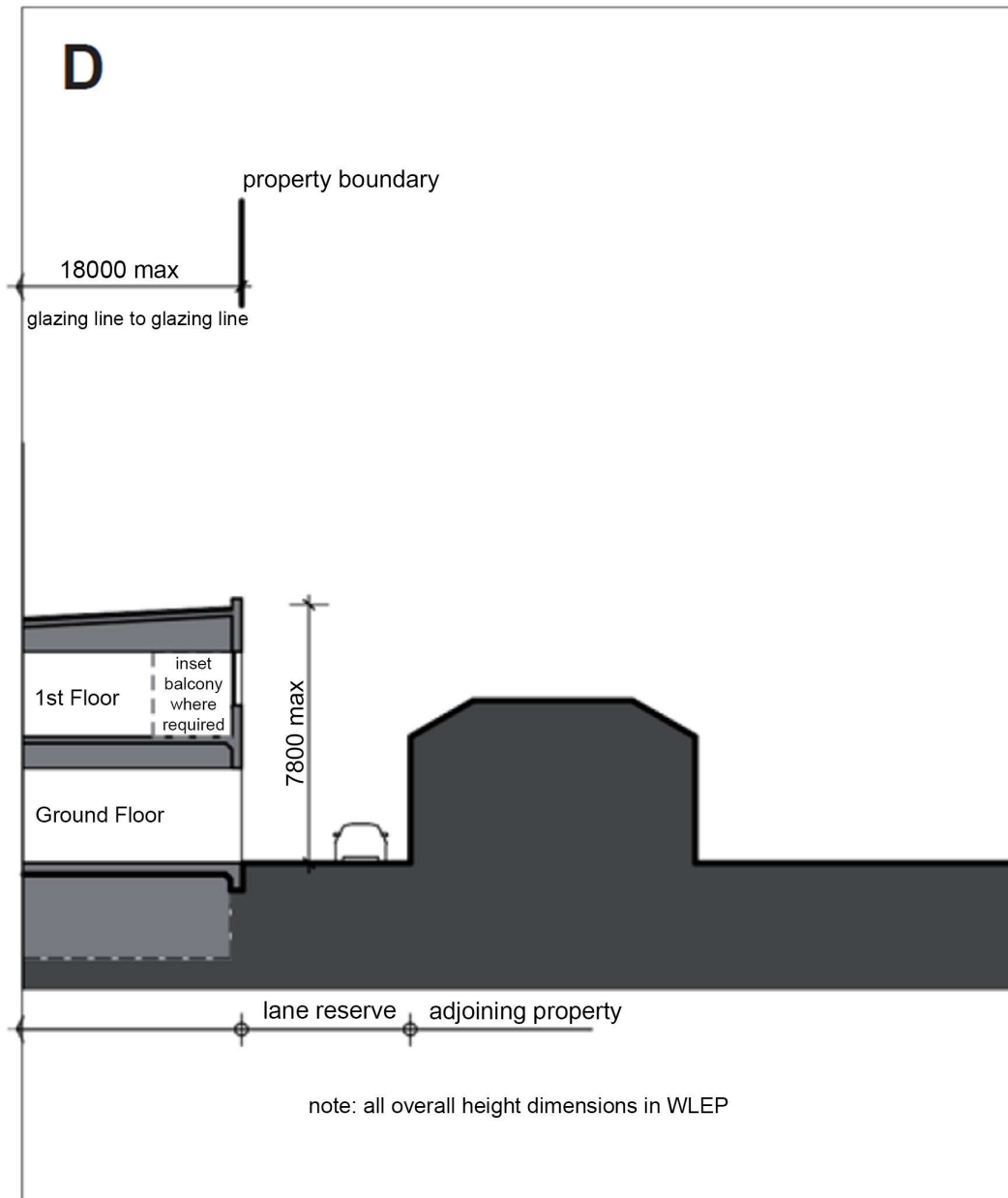




## Two (2) storey detailed rear setback without rear laneway



Two (2) storey detailed section with rear laneway



## ANNEXURE E3-2 TYPICAL BUILT FORM FOR THREE STOREY CENTRES

Annexure E3-2 applies to the following Local Village Centres:

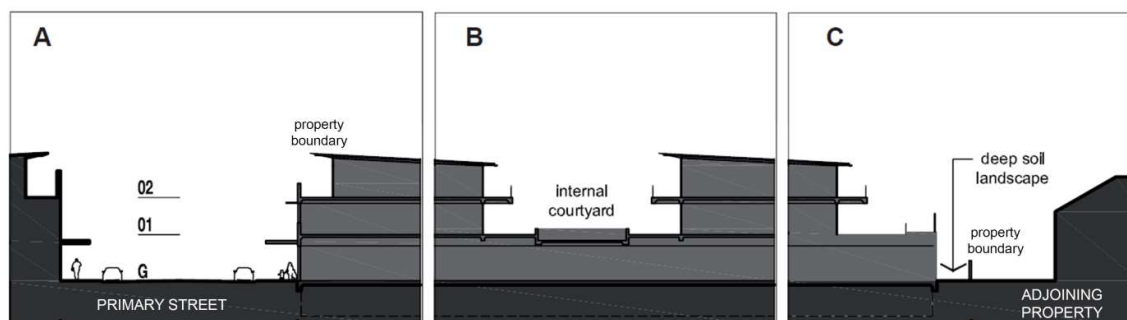
1. Bondi Road Village:
2. Charing Cross Small Village.
3. Rose Bay Small Village (North & South).
4. Glenayr Avenue ~~Neighbourhood~~ Centre.
5. North Bondi ~~Neighbourhood~~ Centre.
6. Blake Street ~~Neighbourhood~~ Centre.
7. Bronte Beach ~~Neighbourhood~~ Centre.
8. Bronte (Macpherson Street) ~~Neighbourhood~~ Centre:
9. Old South Head Road ~~Neighbourhood~~ Centre.

There are two typical built forms for three storey Local Village Centres which are dependent on whether the property has access to a rear lane.

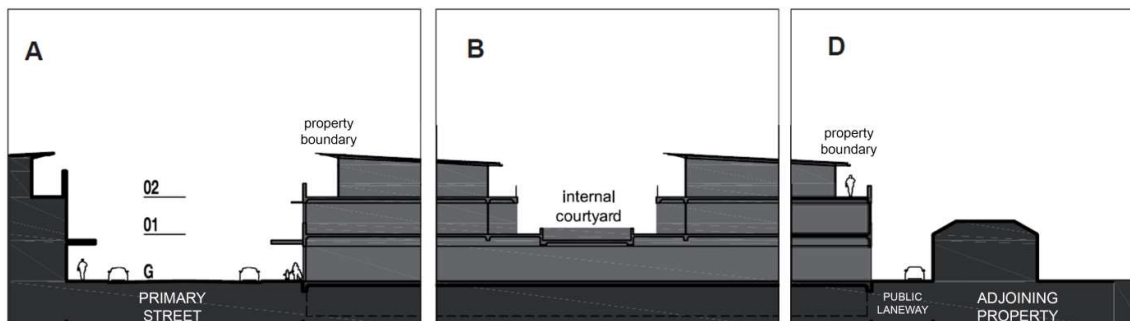
- (a) Properties without rear laneway: Control Diagram A, B and C.
- (b) Properties with rear laneway access: Control Diagrams A, B and D.

For applicable properties refer to the associated maps in Section 3.1 – Specific Controls.

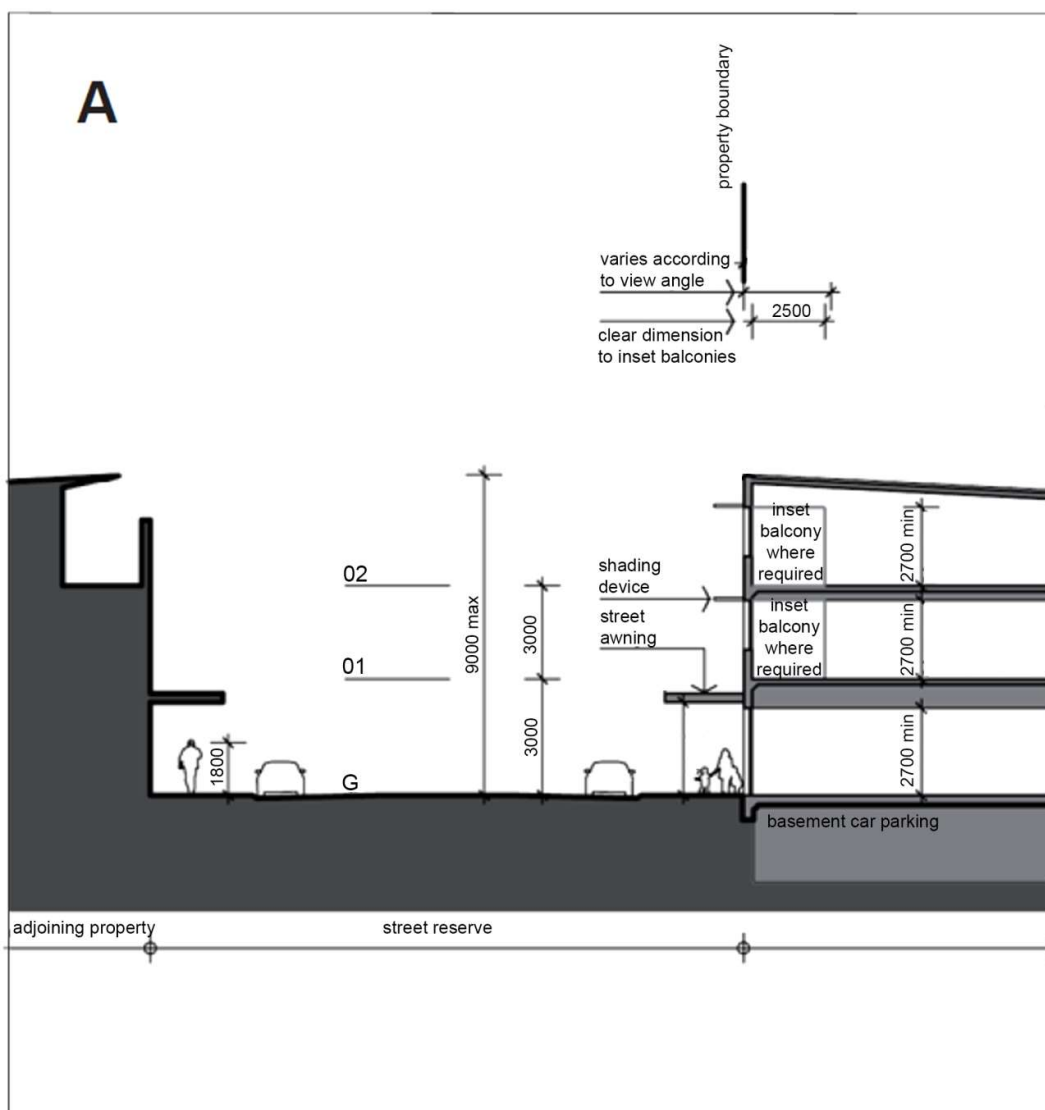
### Three (3) storey section without rear laneway



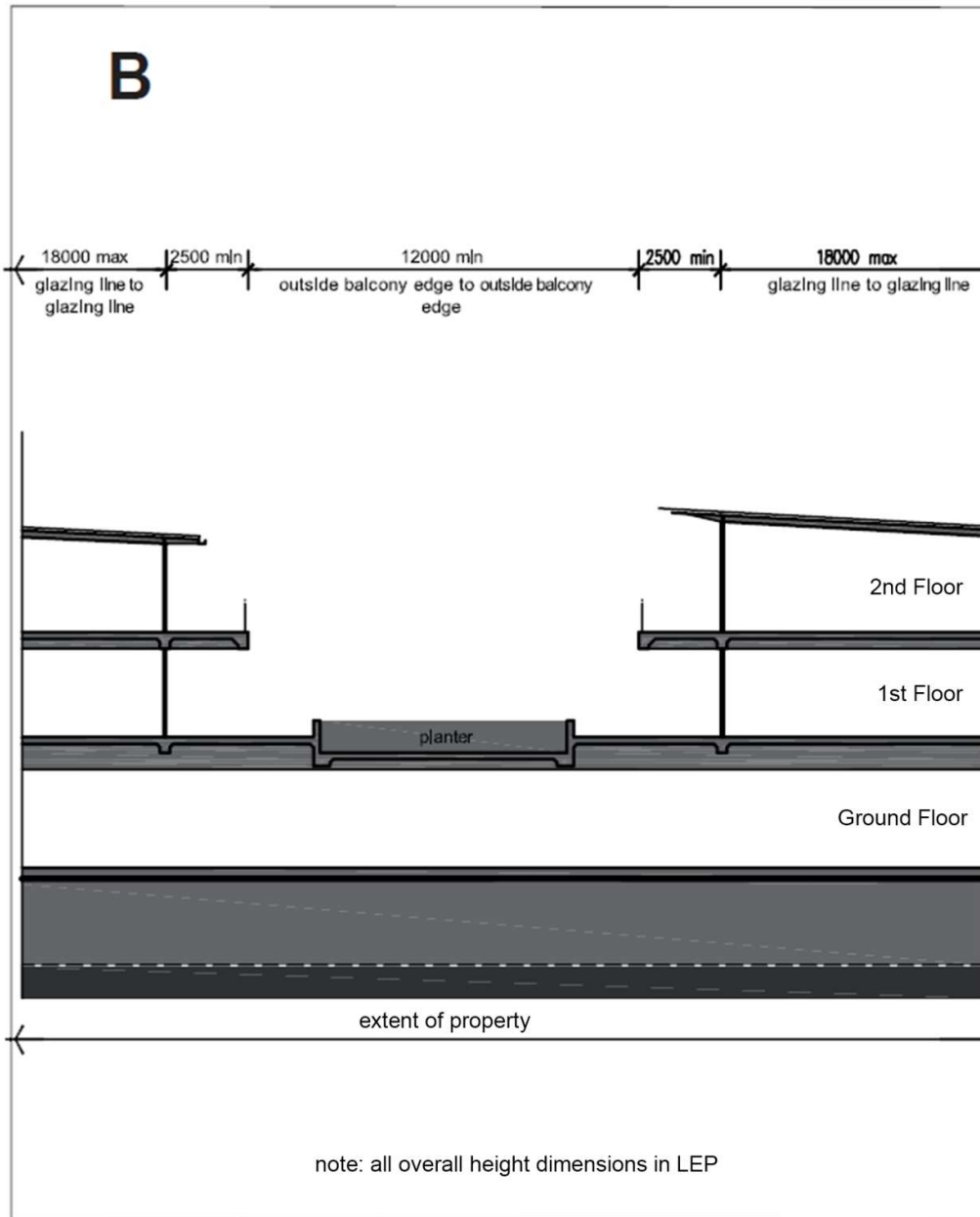
### Three (3) storey section with rear laneway



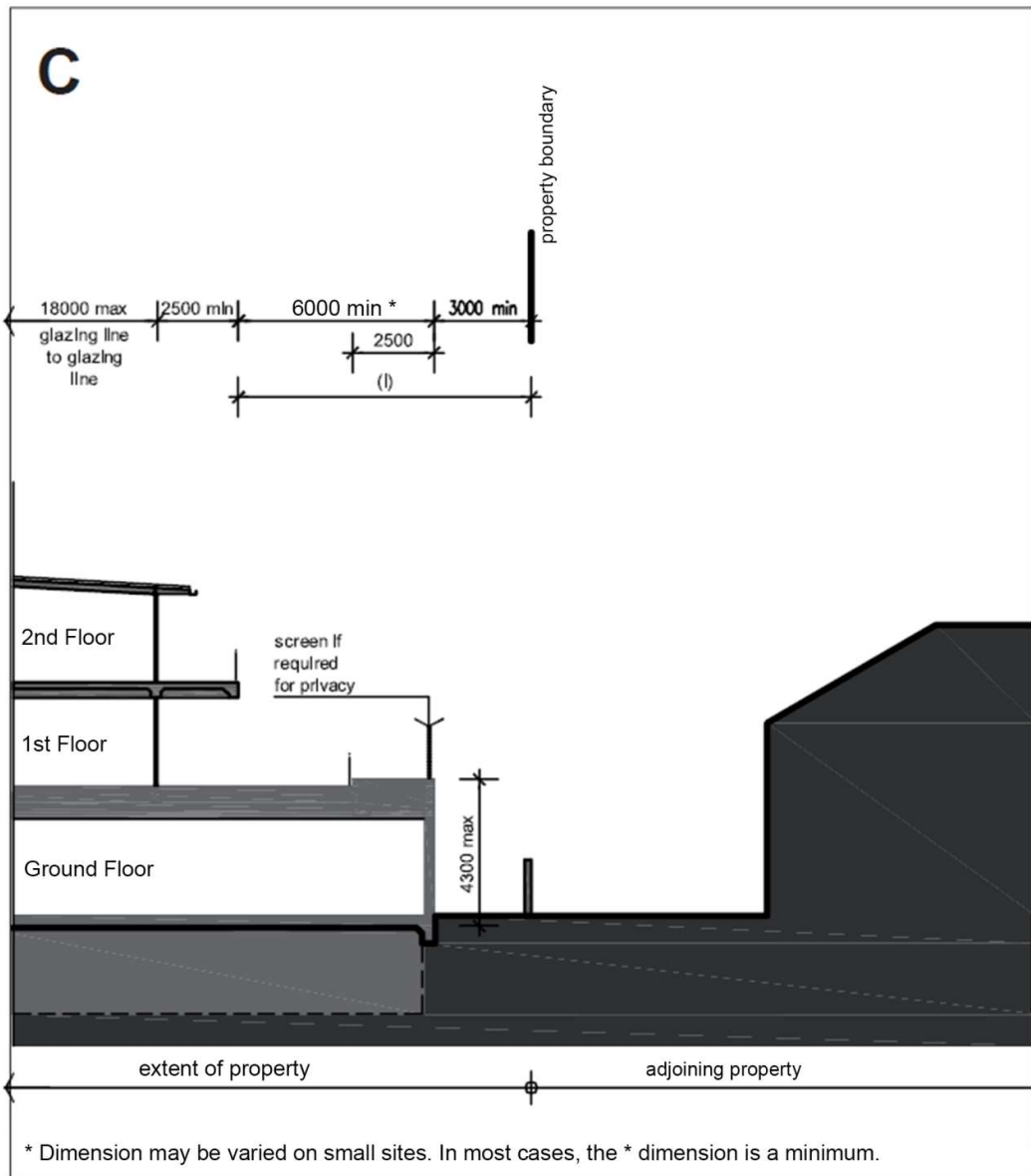
Three (3) storey detailed street interface and internal dimensions



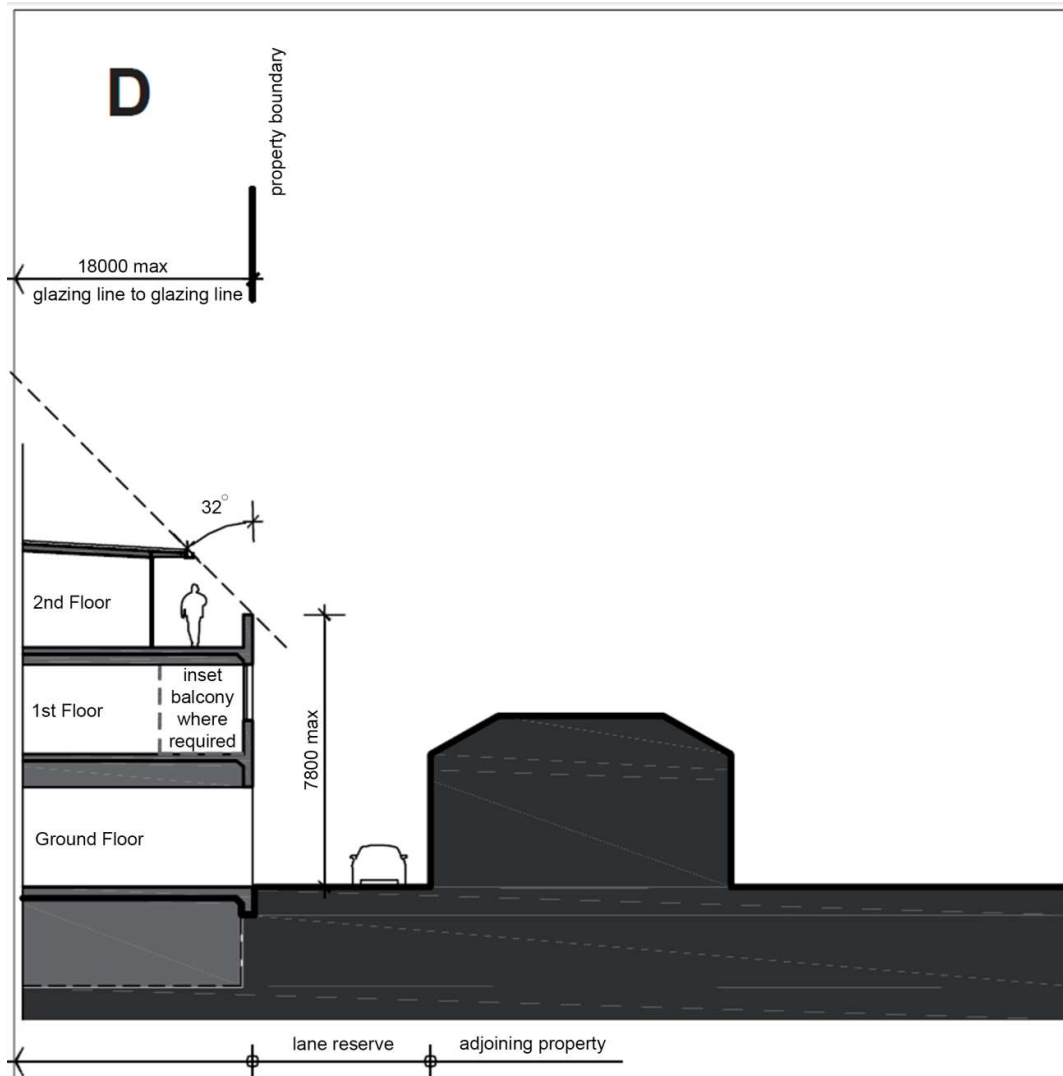
Three (3) storey detailed internal courtyard



Three (3) storey rear setback details without rear laneway



Three (3) storey rear setback details with rear laneway





### ANNEXURE E3-3 TYPICAL BUILT FORM FOR FOUR STOREY CENTRES

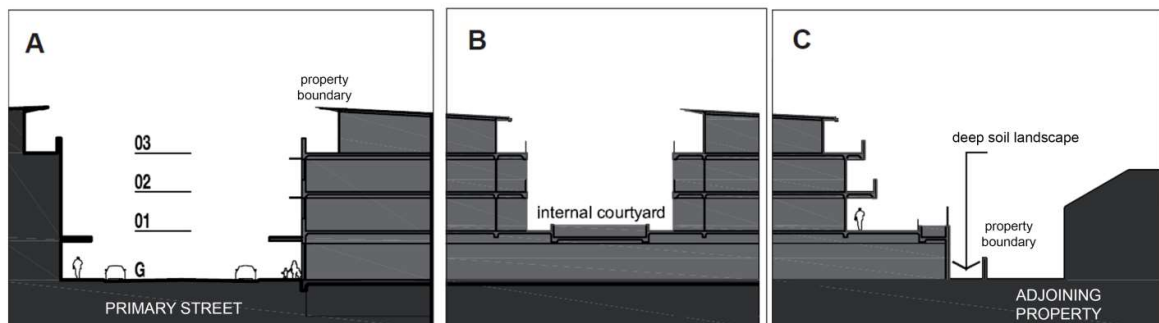
Annexure E3-3 applies to the following centres:

1. Hall Street Town Centre.
2. Bondi Road Village:

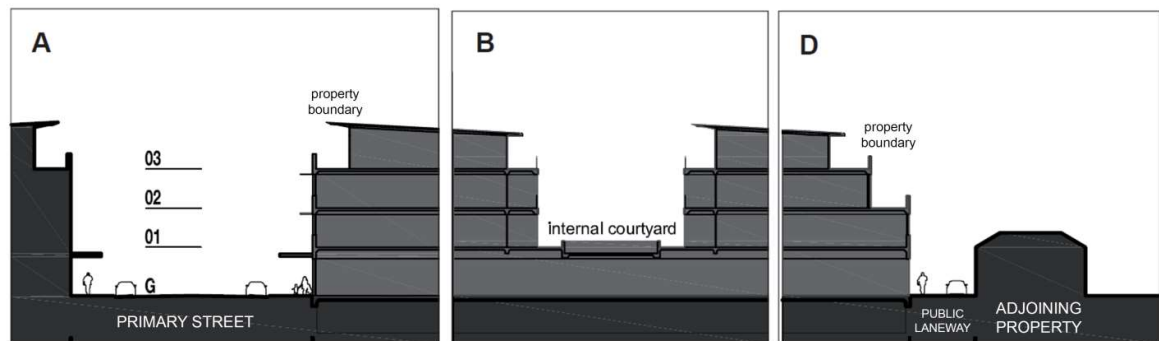
There are two typical built forms for four storey Local Village Centres which are dependent on whether a property has access to a rear lane.

1. Properties without rear laneway: Control Diagram A, B and C.
2. Properties with rear laneway access: Control Diagrams A, B and D.

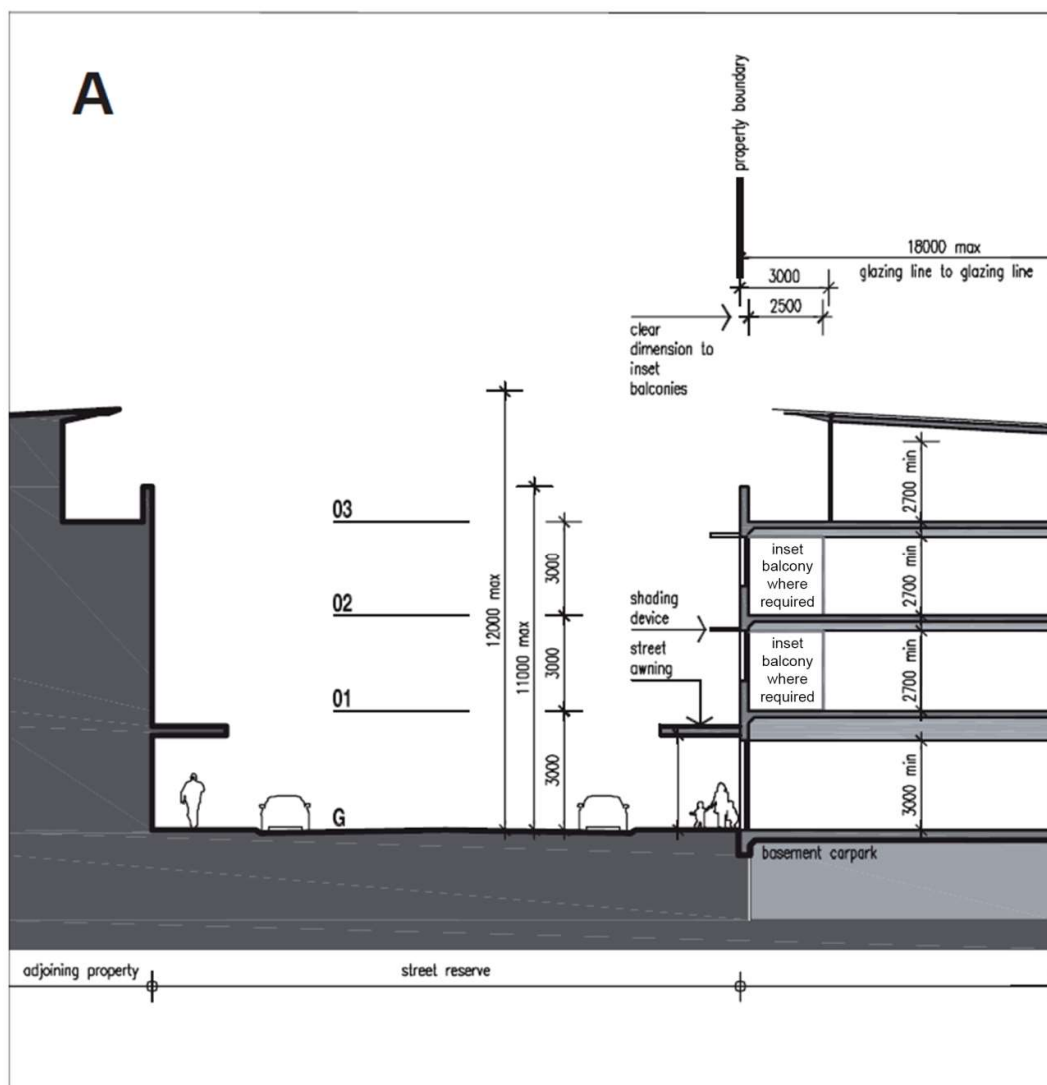
#### Four (4) storey section without rear laneway



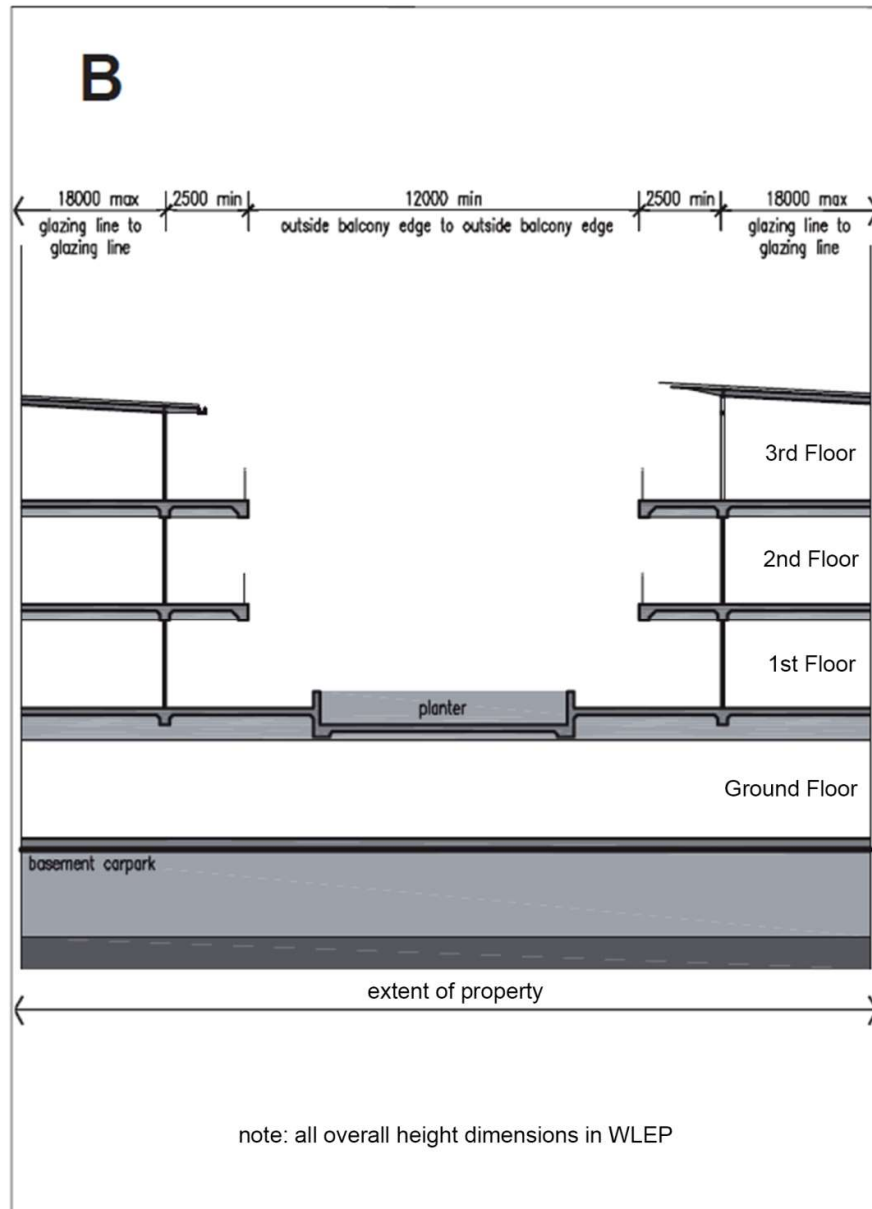
#### Four (4) storey section with rear laneway



Four (4) storey street interface details and internal dimensions

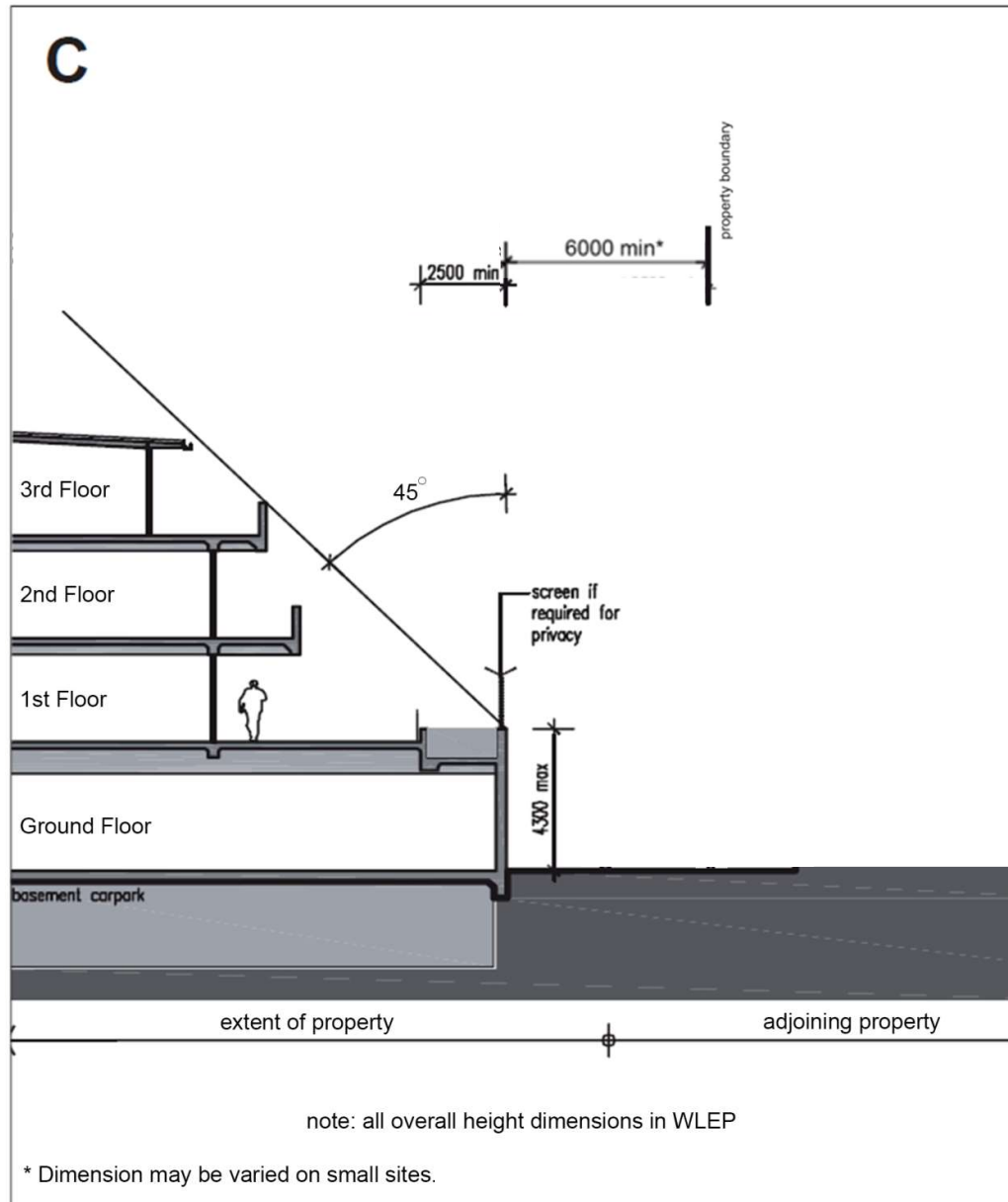


Four (4) storey internal courtyard details



**Four (4) storey rear setback details without rear laneway**

*Diagram: Revised to comply with the Apartment Design Guide which requires a 6m rear setback minimum.*



Four (4) storey rear setback details with rear laneway

